

58 TH STREET NE

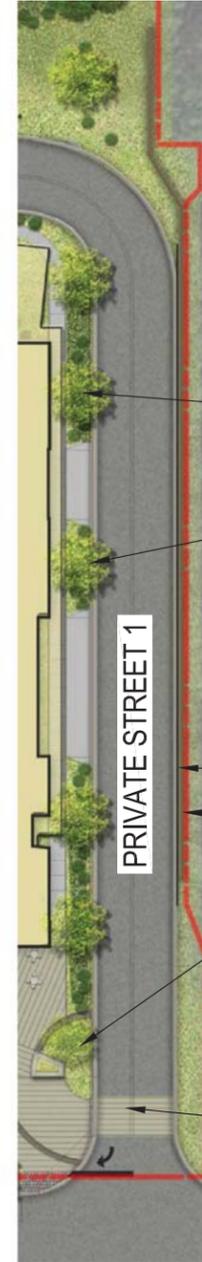
SHADE TREES IN CONTINUOUS 5' WIDE PLANTING VERGE  
 ZELKOVA  
 WILLOW OAK  
 LITTLELEAF LINDEN  
 LIRIOPE UNDERSTORY

BIO RETENTION AREAS WITH SHADE TREES  
 HONEY LOCUST  
 LITTLELEAF LINDEN  
 SWEETSPIRE  
 STAR MAGNOLIA  
 AMELANCHIAR  
 KOUSA DOGWOOD  
 RED AND YELLOW TWIG DOGWOOD  
 SWITCH GRASS  
 WITCH HAZEL  
 LIRIOPE



PRIVATE STREET 2

6' PERVIOUS PAVING SIDEWALK  
 SHADE TREES WITH UNDERSTORY PLANTING  
 HONEY LOCUST  
 HORNBEAM  
 FOUNTAIN GRASS  
 PACHYSANDRA  
 LIRIOPE



PRIVATE STREET 1

SHADE TREES IN LINEAR BIO-FILTRATION AREA REFER TO SHEET L03-02  
 SHADE TREES WITH UNDERSTORY PLANTING  
 HONEY LOCUST  
 HORNBEAM  
 FOUNTAIN GRASS  
 PACHYSANDRA  
 LIRIOPE  
 GUARD RAIL  
 6FT FENCE  
 NATIVE/ ADAPTIVE PLANTING  
 MILKWEED  
 JOE RYE WEED  
 SWITCH GRASS  
 FOUNTAIN GRASS  
 LIRIOPE  
 DAYLILIES  
 KNOCK OUT ROSES  
 DECORATIVE CONCRETE PAVER RAISED CROSSWALK

|   |    |
|---|----|
| STREET TREES PROVIDED AT AVERAGE OF 30 FT ON CENTER | 29 |
|---|----|



APPLICANT  
**A&R COMPANIES**

LAND USE ATTORNEY  
**Holland & Knight**

ARCHITECT - WALMART  
 ARCHITECT - RESTAURANT  
**MMA**

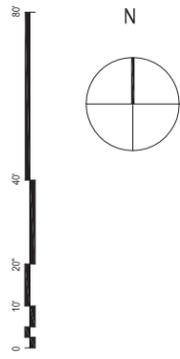
ARCHITECT - MIXED-USE  
**PERKINS + WILL**

LANDSCAPE ARCHITECT  
**cgl**

CIVIL ENGINEER  
 LANDSCAPE ARCHITECT  
**Bowman CONSULTING**

# CAPITOL GATEWAY MARKETPLACE

WASHINGTON, DC



SURFACE PARKING ON-SITE TREE REQUIREMENT (REFER TO SHEET L03-02)

|  |    |
|--|----|
| SITE TREES REQUIRED  | 17 |
| SITE TREES PROVIDED  | 18 |
| 12,970 SF OF OPEN SPACE PROVIDED (20.1% OF GROSS PARCEL)       |    |
| 5,000 SF (OR 20%) OF THE SURFACE PARKING AREA IS POROUS PAVING |    |

SHADE TREES TO ENHANCE CANOPY COVERAGE WITH UNDERSTORY PLANTING REFER TO SHEET L03-02

- HONEY LOCUST
- HORNBEAM
- FOUNTAIN GRASS
- PACHYSANDRA
- LIRIOPE

SHADE TREES WITH UNDERSTORY PLANTING

- HONEY LOCUST
- HORNBEAM
- FOUNTAIN GRASS
- PACHYSANDRA
- LIRIOPE

RAISED PLANTERS REFER TO SHEET L03-02

CONCRETE SIDEWALK

POROUS CONCRETE PAVERS

DECORATIVE CONCRETE PAVING BAND

6FT CONTINUOUS PLANTING VERGE REFER TO SHEET L03-02

- MILKWEED
- JOE RYE WEED
- SWITCH GRASS
- FOUNTAIN GRASS
- LIRIOPE
- DAYLILIES
- KNOCK OUT ROSES

# CAPITOL GATEWAY MARKETPLACE

WASHINGTON, DC

NATIVE/ ADAPTIVE PLANTING SCREEN

- SWEETSPIRE
- STAR MAGNOLIA
- AMELANCHIAR
- KOUSA DOGWOOD
- RED AND YELLOW TWIG DOGWOOD
- SWITCH GRASS
- WITCH HAZEL
- LIRIOPE

GRASS PAVE SYSTEM (POROUS CONCRETE PAVERS COVERING)

RAINGARDENS

- SWEETSPIRE
- STAR MAGNOLIA
- AMELANCHIAR
- KOUSA DOGWOOD
- RED AND YELLOW TWIG DOGWOOD
- SWITCH GRASS
- WITCH HAZEL
- LIRIOPE

EVERGREEN HOLLY SCREEN REFER TO SHEET L03-02



RESTAURANT



LAND USE ATTORNEY  
**Holland & Knight**

ARCHITECT - WALMART  
ARCHITECT - RESTAURANT



ARCHITECT - MIXED-USE

**PERKINS + WILL**  
LANDSCAPE ARCHITECT



CIVIL ENGINEER  
LANDSCAPE ARCHITECT



**CAPITOL GATEWAY  
MARKETPLACE**  
WASHINGTON, DC

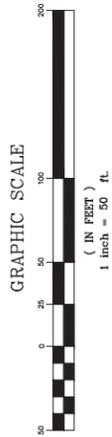
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**05.31.13**

TITLE  
ABBREVIATIONS AND  
LEGEND

SHEET

| EXISTING | DESCRIPTION   | PROPOSED |
|----------|---|----------|
|          | INDEX CONTOUR   |          |
|          | INTERMEDIATE CONTOUR  |          |
|          | EDGE OF PAVEMENT  |          |
|          | CURB AND GUTTER   |          |
|          | TRANSITION FROM CG-6R TO CG-6                                       |          |
|          | PROPOSED HEADER CURB  |          |
|          | PROPERTY LINE   |          |
|          | DEPARTING PROPERTY LINE   |          |
|          | LOT LINE  |          |
|          | RIGHT-OF-WAY  |          |
|          | CENTERLINE  |          |
|          | FLOOD PLAIN   |          |
|          | CLEARING AND GRADING<br>(LIMITS OF DISTURBANCE)                     |          |
|          | TREE LINE   |          |
|          | FLOW LINE OF SWALE  |          |
|          | STREAM  |          |
|          | OVERLAND RELIEF PATHWAY   |          |
|          | FENCE LINE  |          |
|          | EASEMENT  |          |
|          | WATER LINE  |          |
|          | WATER VALVE   |          |
|          | REDUCER   |          |
|          | SANITARY SEWER  |          |
|          | STORM SEWER   |          |
|          | CABLE TV  |          |
|          | ELECTRIC SERVICE  |          |
|          | TELEPHONE SERVICE   |          |
|          | GAS LINE  |          |
|          | OVERHEAD ELECTRIC   |          |
|          | OVERHEAD TELEPHONE  |          |
|          | SPOT ELEVATION  |          |
|          | UTILITY POLE  |          |
|          | SIGN  |          |
|          | SANITARY SEWER IDENTIFIER   |          |
|          | STORM DRAIN IDENTIFIER  |          |
|          | WATER METER   |          |
|          | FIRE HYDRANT  |          |
|          | PARKING INDICATOR<br>INDICATES THE NUMBER OF TYPICAL PARKING SPACES |          |
|          | STREET LIGHT  |          |
|          | VEHICLES PER DAY<br>(TRAFFIC COUNT)                                 |          |
|          | TEST PIT LOCATION<br>RECOMMENDED/REQUIRED                           |          |
|          | CRITICAL SLOPE  |          |
|          | HANDICAP RAMP   |          |
|          | STOP SIGN   |          |
|          | STREET SIGN   |          |
|          | TREE  |          |
|          | BENCHMARK   |          |
|          | ASPHALT TRAIL   |          |
|          | CONCRETE SIDEWALK   |          |
|          | HANDICAP PARKING<br>SPACE (VAN)                                     |          |
|          | RIP RAP   |          |
|          | SPOT GRADE  |          |
|          | ELECTRIC UTILITY TRANSFORMER  |          |

| ABBREVIATIONS | DESCRIPTION  |
|---------------|--|
| A             | AREA OF ARC  |
| AASHTO        | AMERICAN ASSOCIATION OF STATE HWY & TRANSP OFFICIALS |
| AC            | ADJACENT   |
| ADJ           | ADJACENT   |
| AGGR          | AGGREGATE  |
| AHD           | AHEAD  |
| ANSI          | AMERICAN NATIONAL STANDARDS INSTITUTE                |
| APPROX        | APPROXIMATE  |
| ARCH          | ARCHITECTURAL  |
| ASPH          | ASPHALT  |
| ASTM          | AMERICAN SOCIETY FOR TESTING AND MATERIALS           |
| AWWA          | AMERICAN WATER WORKS ASSOCIATION                     |
| B             | BREADTH  |
| BC            | BACK OF CURB   |
| BF            | BASEMENT FLOOR                                       |
| BLDG          | BUILDING   |
| BM            | BENCHMARK  |
| BMP           | BEST MANAGEMENT PRACTICES (WATER QUALITY)            |
| BOV           | BLOW OFF VALVE                                       |
| BRG           | BEARING  |
| BRL           | BUILDING RESTRICTION LINE                            |
| BVCE          | BEGINNING VERTICAL CURVE ELEVATION                   |
| BVCS          | BEGINNING VERTICAL CURVE STATION                     |
| BW            | BOTTOM OF WALL                                       |
| c.e           | CENTER CORRECTION ON VERTICAL CURVE                  |
| C             | RUNOFF COEFFICIENT                                   |
| C&G           | CABLE TELEVISION                                     |
| CATV          | CURB AND GUTTER                                      |
| CB            | CATCH BASIN  |
| CBR           | CALIFORNIA BEARING RATIO                             |
| CC            | CENTER TO CENTER                                     |
| CF            | CUBIC FEET   |
| CFS           | CUBIC FEET PER SECOND                                |
| CG(R)         | CURB AND GUTTER (REVERSE SLOPE)                      |
| CH            | CHORD  |
| CHBRG         | CHORD BEARING  |
| CIP           | CAST IRON PIPE                                       |
| CL            | CENTERLINE OR CLASS                                  |
| C/L           | CENTERLINE   |
| CLR           | CLEAR  |
| CM            | CUBIC METERS   |
| CMP           | CORRUGATED METAL PIPE                                |
| CMS           | CUBIC METERS PER SECOND                              |
| CN            | RUNOFF CURVE NUMBER                                  |
| CONT          | CONTINUOUS   |
| CO            | CLEAN OUT  |
| CONC          | CONCRETE   |
| CS            | CURB STOP  |
| CT            | COURT  |
| CTR           | CENTERLINE   |
| CY            | CUBIC YARD   |
| D             | DEPTH  |
| DA            | DRAINAGE AREA  |
| DB            | DEED BOOK  |
| DET           | DETAIL   |
| DIA           | DIAMETER   |
| DIP           | DUCTILE IRON PIPE                                    |
| DI            | DROP INLET   |
| DIST          | DISTANCE   |
| DL            | DOMESTIC LINE  |
| DM            | DROP MANHOLE   |
| DOM           | DOMESTIC   |
| DR            | DRIVE  |
| DRN           | DRAINAGE AREA  |
| DS            | DOWN SPOUT   |
| DJ            | DWELLING UNITS                                       |
| DWG           | DRAWING  |
| D/W           | DRIVEWAY   |
| Δ             | DELTA  |
| E             | RATE OF SUPER ELEVATION                              |
| EA            | EACH   |
| EBL           | EAST BOUND LANE                                      |
| EC            | EROSION CONTROL                                      |
| EG            | EDGE OF GUTTER                                       |
| EGL           | ENERGY GRADIENT LINE                                 |
| EL            | ELEVATION  |
| ELEC          | ELECTRIC   |
| ELEV          | ELEVATION  |
| ENGR          | ENGINEER   |
| ENT           | ENTRANCE   |
| EP            | EDGE OF PAVEMENT                                     |
| EQUIP         | EQUIPMENT  |
| ES            | END SECTION  |
| ESMT          | EASEMENT   |
| ETD           | EXISTING TO BE DEMOLISHED                            |
| ETR           | EXISTING TO REMAIN                                   |
| ETRL          | EXISTING TO BE RELOCATED                             |
| ETRP          | EXISTING TO BE REPLACED                              |
| EVCE          | ENDING VERTICAL CURVE ELEVATION                      |
| EVCS          | ENDING VERTICAL CURVE STATION                        |
| EW            | END WALL   |
| EX            | EXISTING   |
| EQC           | ENVIRONMENTAL QUALITY CORRIDOR                       |
| F             | FIRE LINE  |
| FAR           | FLOOR AREA RATIO                                     |
| FC            | FACE OF CURB   |
| FD            | FLOOR DRAIN  |
| FF            | FIRST FLOOR  |
| FG            | FINISH GRADE   |
| FH            | FIRE HYDRANT   |
| FL            | FLOW LINE  |
| FND           | FOUNDATION   |
| FOY           | FOYER  |
| FP            | FLOOD PLAIN  |
| FPS           | FEET PER SECOND                                      |
| FS            | FIRE SERVICE OR FACTOR OF SAFETY                     |
| FT            | FOOT / FEET  |
| G             | GAS  |
| GAR           | GARAGE   |
| GFA           | GROSS FLOOR AREA                                     |
| GR            | GUARD RAIL OR GRATE INLET                            |
| H             | HEAD   |
| HC            | HANDICAP   |
| HB            | HORIZONTAL BEND                                      |
| HGL           | HYDRAULIC GRADE LINE                                 |
| HORZ          | HORIZONTAL   |
| HP            | HIGH POINT   |
| HR            | HAND RAIL  |
| HT            | HEIGHT   |
| HW            | HEADWATER  |
| I             | RAINFALL INTENSITY                                   |
| ID            | INSIDE DIAMETER OR IDENTIFICATION                    |
| IE            | INVERT ELEVATION                                     |
| IN            | INCH   |
| INV           | INVERT   |
| IP            | IRON PIPE  |
| IPF           | IRON PIPE FOUND                                      |
| IPS           | IRON PIPE SET  |
| JB            | JUNCTION BOX   |
| JNT           | JOINT  |
| K             | SIGHT DISTANCE COEFFICIENT                           |
| Ke            | CULVERT ENTRANCE LOSS COEFFICIENT                    |
| L             | LENGTH   |
| LAT           | LATERAL  |
| LCG           | LIMITS OF CLEARING & GRADING                         |
| LF            | LINEAR FEET  |
| LL            | LOWER LEVEL  |
| LOS           | LINE OF SIGHT  |
| LP            | LOW POINT  |
| LS            | LOADING SPACE  |
| LT            | LEFT   |
| M             | MONUMENT FOUND                                       |
| MAX           | MAXIMUM  |
| MECH          | MECHANICAL   |
| MH            | MANHOLE  |
| MI            | MILE   |
| MIN           | MINIMUM  |
| MISC          | MISCELLANEOUS  |
| MPH           | MILES PER HOUR                                       |
| MS            | MEDIAN STRIP   |
| MSL           | MEAN SEA LEVEL                                       |
| NA OR N/A     | NOT APPLICABLE                                       |
| NBL           | NORTH BOUND LANE                                     |
| N/F           | NOW OR FORMERLY                                      |
| NFA           | NET FLOOR AREA                                       |
| NO. OR #      | NUMBER   |
| OC            | ON CENTER  |
| OBJ           | OBJECT   |
| OD            | OUTSIDE DIAMETER                                     |
| OH            | OVERHANG   |
| O/H           | OVERHEAD   |
| OHC           | OVERHEAD CABLE                                       |
| OHE           | OVERHEAD ELECTRIC                                    |
| OHT           | OVERHEAD TELEPHONE                                   |
| P             | PERIMETER  |
| P&P           | PLAN AND PROFILE                                     |
| PC            | POINT OF CURVATURE                                   |
| PCC           | POINT OF COMPOUND CURVE                              |
| PCTC          | POINT OF CURVATURE TOP OF CURB                       |
| PCEP          | POINT OF CURVE EDGE OF PAVEMENT                      |
| PFM           | PUBLIC FACILITIES MANUAL                             |
| PG            | PAGE   |
| PGL           | POINT OF GRADE LINE                                  |
| PI            | POINT OF INTERSECTION                                |
| PL            | PROPERTY LINE  |
| PL            | PROPERTY LINE  |
| PRC           | POINT OF REVERSE CURVE                               |
| PRELIM        | PRELIMINARY  |
| PROP          | PROPOSED   |
| PT            | POINT OF TANGENCY                                    |
| PVC           | POINT OF VERTICAL CURVE                              |
| PVI           | POINT OF VERTICAL INTERSECTION                       |
| PVMT          | PAVEMENT   |
| PVRC          | POINT OF VERTICAL REVERSE CURVE                      |
| PVT           | POINT OF VERTICAL TANGENT                            |
| Q (cfs)       | AMOUNT OF RUNOFF (FLOW RATE)                         |
| R             | RADIUS   |
| RCP           | REINFORCED CONCRETE PIPE                             |
| RD            | ROAD OR ROOF DRAIN                                   |
| REINF         | REINFORCED   |
| REQD          | REQUIRED   |
| RET           | RETAINING  |
| REV           | REVISION   |
| RGP           | ROUGH GRADING PLAN                                   |
| RMA           | RESOURCE MANAGEMENT AREA                             |
| ROM           | REMOTE OUTSIDE MONITOR                               |
| RPA           | RESOURCE PROTECTION AREA                             |
| RR            | RAILROAD   |
| RT            | RIGHT  |
| RTE           | ROUTE  |
| R/W           | RIGHT OF WAY   |
| S             | SPEED OR SLOPE                                       |
| SAN           | SANITARY   |
| SBL           | SOUTH BOUND LANE                                     |
| SCH           | SCHEDULE   |
| SD            | SIGHT DISTANCE                                       |
| SEC           | SECTION  |
| SECT          | SECTION  |
| SEW           | SEWER  |
| SF            | SQUARE FEET  |
| SH            | SHOULDER   |
| SP            | SPACE OR SITE PLAN                                   |
| SPEC          | SPECIFICATIONS                                       |
| STA           | STATION  |
| STD           | STANDARD   |
| STK           | STACK  |
| STM           | STORM  |
| STR           | STRUCTURE  |
| SVC           | SERVICE  |
| S/W           | SIDEWALK   |
| SWM           | STORM WATER MANAGEMENT                               |
| Sx            | CROSS SLOPE  |
| SY            | SQUARE YARD  |
| T             | TANGENT  |
| TB            | TOP OF BANK OR TEST BORING                           |
| TC            | TOP OF CURB  |
| Tc            | TIME OF CONCENTRATION                                |
| TEL           | TELEPHONE  |
| TEMP          | TEMPORARY  |
| TH            | TEST HOLE  |
| TP            | TEST PIT OR TREE PROTECTION                          |
| TRANSP        | TRANSPORTATION                                       |
| TW            | TOP OF WALL OR TAILWATER                             |
| TYP           | TYPICAL  |
| UG            | UNDERGROUND  |
| UGE           | UNDERGROUND ELECTRIC                                 |
| UGC           | UNDERGROUND CABLE                                    |
| UL            | UPPER LEVEL  |
| UP            | UTILITY POLE   |
| USGS          | US GEOLOGICAL SURVEY                                 |
| V OR VOL      | VOLUME   |
| V OR VEL      | VELOCITY   |
| VA            | VIRGINIA   |
| VAN           | HANDICAPPED VAN PARKING SPACE                        |
| VB            | VERTICAL BEND  |
| VC            | VERTICAL CURVE                                       |
| VF            | VERTICAL FOOT  |
| W             | WEIGHT OR WIDTH                                      |
| WBL           | WEST BOUND LANE                                      |
| WL            | WATER LINE   |
| WM            | WATER METER  |
| W/M OR WM     | WATER MAIN   |
| WQA           | WATER QUALITY IMPACT ASSESMENT                       |
| WV            | WATER VALVE  |
| XF            | TRANSFORMER  |
| YI            | YARD INLET   |
| YR            | YEAR   |
| Z             | SIDE SLOPES  |



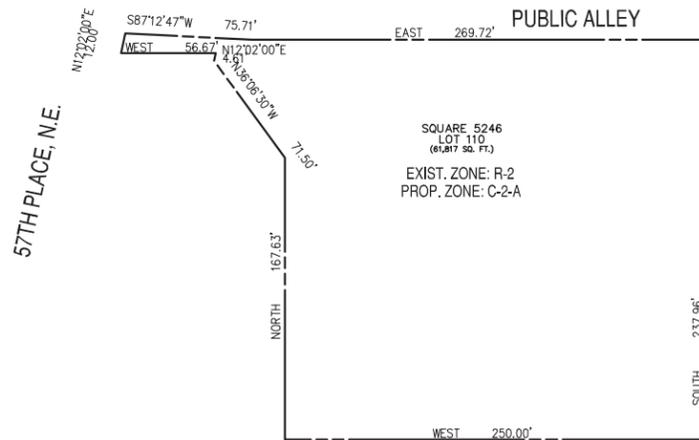
**\*EAST PARCEL TABULATIONS**

|                        |              |                     | EX. ZONE | PROP. ZONE |
|------------------------|--------------|---------------------|----------|------------|
| SQUARE 5272:           | LOT 51       | 25,718 S.F.         | R-5-A    | C-2-A      |
| SQUARE 5273:           | LOT 67       | 56,141 S.F.         | R-5-A    | C-2-A      |
| SQUARE 5276:           | AT&T LOT 812 | 114,509 S.F.        | R-5-A    | C-2-A      |
|                        | AT&T LOT 813 | 25,340 S.F.         | R-5-A    | C-2-A      |
|                        | LOTS 23-121  | 206,104 S.F.        | R-5-A    | C-2-A      |
| SQUARE 5277:           | LOTS 22-33   | 23,353 S.F.         | R-5-A    | C-2-A      |
|                        | AT&T LOT 805 | 8,752 S.F.          | R-5-A    | C-2-A      |
| <b>TOTAL SITE AREA</b> |              | <b>459,917 S.F.</b> |          |            |

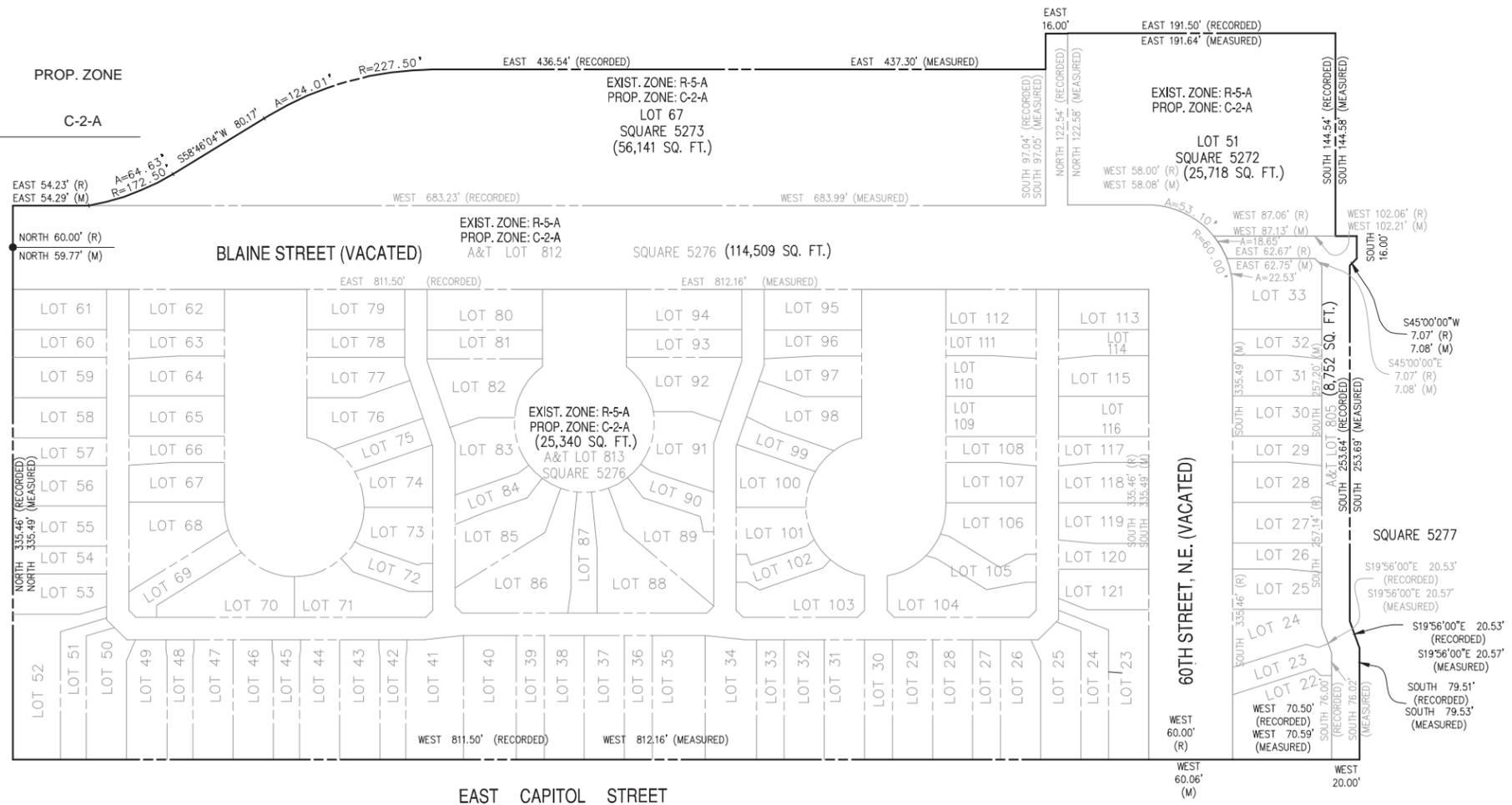
**\*WEST PARCEL TABULATIONS**

|                        |         |                    | EX. ZONE | PROP. ZONE |
|------------------------|---------|--------------------|----------|------------|
| SQUARE 5246:           | LOT 110 | 61,817 S.F.        | R-2      | C-2-A      |
| <b>TOTAL SITE AREA</b> |         | <b>61,817 S.F.</b> |          |            |

\*EAST AND WEST OF 58TH ST. N.E.



**WEST PARCEL**



**EAST PARCEL**



APPLICANT  
**A&R COMPANIES**

LAND USE ATTORNEY  
**Holland & Knight**

ARCHITECT - WALMART  
 ARCHITECT - RESTAURANT  
**MMA**

ARCHITECT - MIXED-USE  
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**cgl**

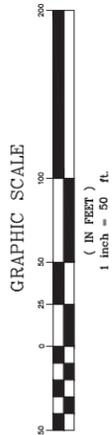
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**Bowman CONSULTING**

**CAPITOL GATEWAY  
 MARKETPLACE**  
 WASHINGTON, DC



# DEMOLITION NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSAL (IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES) ALL STRUCTURES, PADS, WALLS, FLUMES, FOUNDATIONS, PARKING, DRIVES, DRAINAGE, STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL PER THE SPECIFICATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
3. THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY'S FORCES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
4. THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS OF EXISTING UTILITIES.
5. ALL EXISTING SEWERS, PIPING AND UTILITIES SHOWN ARE NOT TO BE INTERRUPTED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTIPOATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE PROCEEDING WITH THE WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE GROUTED IF UNDER BUILDING.
6. ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE. CONTRACTOR SHALL PAY CLOSE ATTENTION TO EXISTING UTILITIES WITHIN THE ANY ROAD RIGHT OF WAY DURING CONSTRUCTION.
7. CONTRACTOR MUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC., (AND OTHER APPROPRIATE BEST MANAGEMENT PRACTICES) AS APPROVED BY CONSTRUCTION MANAGER.
8. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.
9. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
10. SHOULD REMOVAL AND/OR RELOCATION ACTIVITIES DAMAGE STRUCTURES, THE CONTRACTOR SHALL PROVIDE NEW MATERIALS/STRUCTURES IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. EXCEPT FOR MATERIALS DESIGNED TO BE RELOCATED ON THIS PLAN, ALL OTHER CONSTRUCTION MATERIALS SHALL BE NEW.
11. CONTRACTOR MAY LIMIT SAW-CUT & PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR.
12. THE CONTRACTOR SHALL COORDINATE WATER MAIN WORK WITH THE FIRE DEPT. AND THE CITY/COUNTY UTILITY DEPARTMENT TO PLAN PROPOSED IMPROVEMENTS. CONTRACTOR WILL BE RESPONSIBLE FOR ARRANGING/PROVIDING ANY REQUIRED WATER MAIN SHUT OFFS WITH THE CITY/COUNTY DURING CONSTRUCTION. ANY COSTS ASSOCIATED WITH WATER MAIN SHUT OFFS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO EXTRA COMPENSATION WILL BE PROVIDED.
13. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT CONTRACTOR'S EXPENSE.



### EXISTING SANITARY SEWER TABULATION

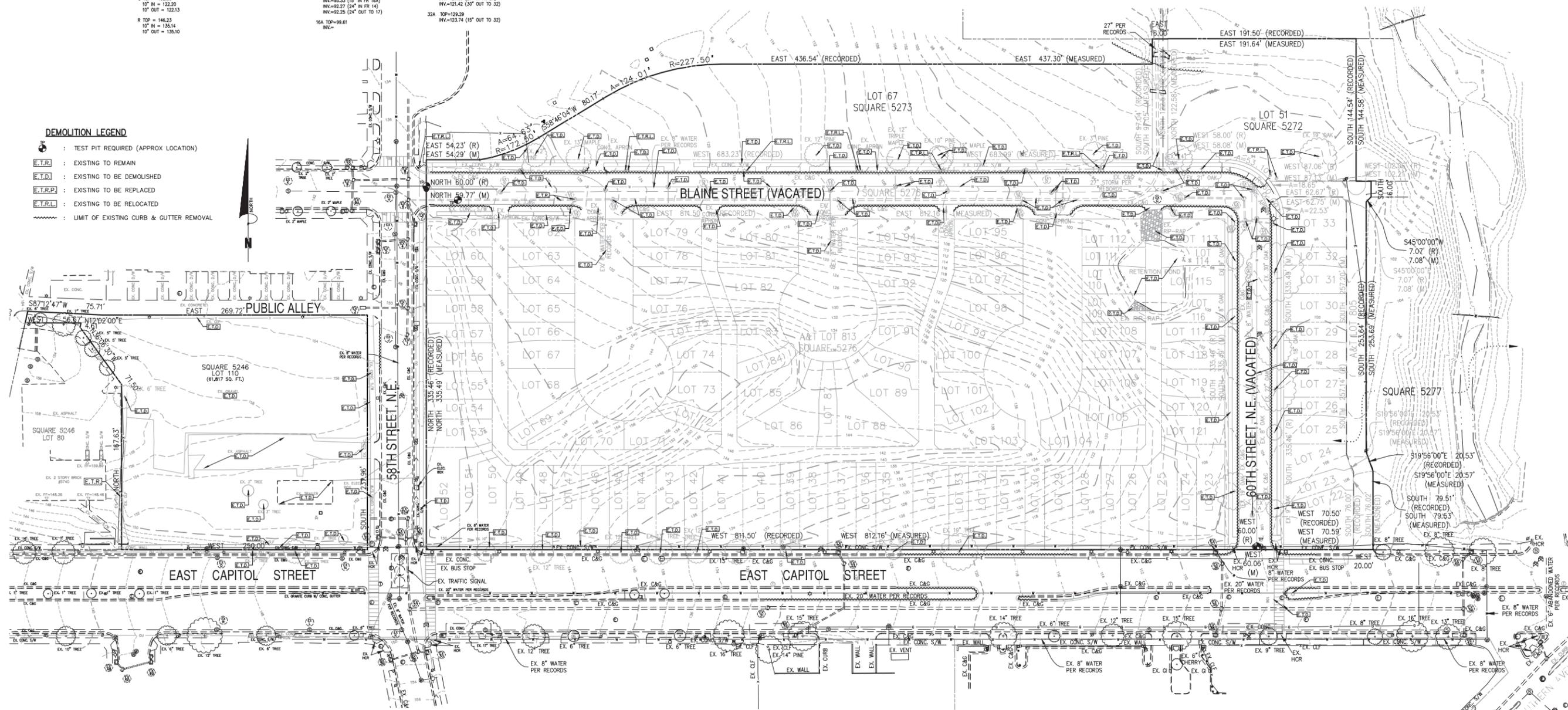
|   |   |
|---|---|
| A | TOP=106.38<br>INV.=97.1 (10' OUT TO B)  |
| B | TOP=94.76<br>INV.=84.9 (10' IN FR A)<br>INV.=84.44 (10' OUT TO C)                             |
| C | TOP=91.92<br>INV.=81.26 (10' IN FR B)<br>INV.=81.10 (10' IN FR C)<br>INV.=81.01 (10' OUT)     |
| D | TOP=87.33<br>INV.=87.27 (10' OUT TO C)<br>INV.=87.11 (10' IN FR E)                            |
| E | TOP=103.94<br>INV.=93.64 (10' IN FR F)<br>INV.=94.18 (10' IN)<br>INV.=93.58 (10' OUT TO D)    |
| F | TOP=119.71<br>INV.=109.45 (10' OUT TO E)<br>INV.=109.59 (10' IN FR G)                         |
| G | TOP=133.78<br>INV.=123.15 (10' IN)<br>INV.=122.99 (10' OUT TO F)                              |
| H | TOP=140.14<br>INV.=139.88 (10' IN FR I)<br>INV.=139.39 (10' IN FR K)<br>INV.=129.84 (10' OUT) |
| I | TOP=143.77<br>INV.=133.71 (10' IN FR J)<br>INV.=132.70 (10' OUT TO H)                         |
| J | TOP=152.94<br>INV.=142.53 (10' IN)<br>INV.=142.53 (10' OUT TO I)                              |
| K | TOP = 148.42<br>10' IN = 139.28 (PER RECORDS)   |
| L | TOP = 146.23<br>10' IN = 135.15<br>10' OUT = 135.10   |
| M | TOP = 130.36<br>10' IN = 119.06<br>10' OUT = 119.04   |
| N | TOP = 149.11<br>10' OUT = 139.19  |
| O | TOP = 146.16<br>10' IN = 135.46<br>10' OUT = 135.45   |
| P | TOP = 131.29<br>10' IN = 123.98 (FR. Q)<br>10' IN = 120.88 (FR. Q)<br>10' OUT = 120.75        |
| Q | TOP = 132.55<br>10' IN = 122.20<br>10' OUT = 122.13   |
| R | TOP = 146.23<br>10' IN = 135.14<br>10' OUT = 135.10   |

### EXISTING STORM SEWER TABULATION

|     |   |
|-----|---|
| 1   | TOP=151.87<br>INV.=151.87 (SUBBED W/CAP @ MH)<br>INV.=145.08 (15' OUT TO 2)   |
| 2   | TOP=149.83<br>INV.=142.28 (15' OUT TO 4)<br>INV.=143.00 (15' FR 3)<br>INV.=142.28 (15' OUT TO 4)                                      |
| 3   | TOP=151.13<br>INV.=144.45 (15' OUT TO 2)  |
| 4   | TOP=143.54<br>INV.=136.76 (15' IN FR 5)<br>INV.=136.54 (15' IN FR 2)<br>INV.=136.09 (15' OUT TO 6)                                    |
| 5   | TOP (3 MH, CB)=144.77<br>INV.=138.37 (15' OUT TO 4)   |
| 6   | TOP=140.16<br>INV.=133.19 (15' IN FR 11)<br>INV.=133.30 (15' IN FR 4)<br>INV.=132.94 (15' OUT TO 8)                                   |
| 7   | TOP (CB)=141.86<br>INV.=FILLED  |
| 8   | TOP=138.65<br>INV.=131.11 (15' IN FR 6)<br>INV.=131.37 (15' IN FR 9)<br>INV.=130.81 (15' OUT)   |
| 9   | TOP=140.38<br>INV.=136.71 (15' IN FR 9A)<br>INV.=136.11 (15' IN FR 10)<br>INV.=133.33 (15' OUT TO 8)                                  |
| 9A  | TOP=140.98<br>INV.=137.03 (15' OUT TO 9)  |
| 10  | TOP (CB)=140.86<br>INV.=136.20 (15' OUT TO 9)   |
| 11  | TOP=128.77<br>INV.=124.68 (12' IN FR 12)<br>INV.=122.15 (15' IN FR 8)<br>INV.=121.92 (15' OUT TO 13)                                  |
| 12  | TOP (CB)=129.33<br>INV.=FILLED  |
| 13  | TOP (CB)=126.46<br>INV.=119.49 (15' IN FR 11)<br>INV.=119.48 (15' OUT TO 14)  |
| 14  | TOP=111.44<br>INV.=104.71 (15' IN FR 13)<br>INV.=103.95 (24' OUT TO 16)   |
| 15  | TOP (CB)=111.73<br>INV.=FILLED (15' OUT TO 14)  |
| 16  | TOP=98.96<br>INV.=85.33 (15' IN FR 16A)<br>INV.=82.27 (14' IN FR 14)<br>INV.=82.25 (24' OUT TO 17)                                    |
| 16A | TOP=99.61<br>INV.=  |
| 17  | TOP=86.45<br>INV.=85.50 (24' IN FR 16)<br>INV.=86.76 (12' IN)<br>INV.=86.38 (12' OUT)   |
| 18  | TOP (CB)=91.86<br>INV.=FILLED   |
| 19  | TOP=82.16<br>INV.=84.64 (15' IN)<br>INV.=88.05 (12' IN FR 18)<br>INV.=84.36 (18' OUT TO 19A)  |
| 19A | TOP=91.55<br>INV.=FILLED  |
| 20  | TOP=95.07<br>INV.=87.57 (15' IN FR 21)<br>INV.=87.46 (15' OUT TO 19)  |
| 21  | TOP=95.85<br>INV.=89.24 (15' IN)<br>INV.=91.87 (15' IN FR 22)<br>INV.=91.46 (15' IN FR 23)<br>INV.=88.47 (15' OUT TO 20)              |
| 22  | TOP=95.84<br>INV.=FILLED  |
| 23  | TOP=96.08<br>INV.=FILLED  |
| 24  | TOP=150.03<br>INV.=FILLED   |
| 25  | TOP=149.69<br>INV.=141.58 (18' IN FR 25-A)<br>INV.=145.22 (15' IN FR 26)<br>INV.=145.46 (18' OUT TO 27)<br>INV.=145.67 (15' IN FR 24) |
| 25A | TOP=150.29<br>INV.=141.97 (18' OUT TO 25)<br>INV.=141.96 (15' IN)   |
| 26  | TOP (CB)=149.90<br>INV.=FILLED  |
| 27  | TOP=149.42<br>INV.=140.54 (18' IN FR 25)<br>INV.=139.79 (24' IN FR 28)<br>INV.=139.72 (27' TO 31)                                     |
| 28  | TOP=150.79<br>INV.=143.46 (24' IN)<br>INV.=145.64 (15' IN FR 29)<br>INV.=147.18 (15' IN FR 30)<br>INV.=143.15 (24' OUT TO 27)         |
| 29  | TOP=151.64<br>INV.=FILLED   |
| 30  | TOP=151.20<br>INV.=FILLED   |
| 31  | TOP=130.71<br>INV.=121.55 (27' IN FR 31)<br>INV.=121.42 (30' OUT TO 32)   |
| 32A | TOP=129.29<br>INV.=123.74 (15' OUT TO 32)   |

### DEMOLITION LEGEND

- : TEST PIT REQUIRED (APPROX LOCATION)
- : EXISTING TO REMAIN
- : EXISTING TO BE DEMOLISHED
- : EXISTING TO BE REPLACED
- : EXISTING TO BE RELOCATED
- : LIMIT OF EXISTING CURB & GUTTER REMOVAL



# CAPITOL GATEWAY MARKETPLACE

WASHINGTON, DC

DATE  
05.31.13

TITLE  
EXISTING CONDITIONS AND DEMO PLAN

SHEET



LAND USE ATTORNEY  
**Holland & Knight**

ARCHITECT - WALMART  
ARCHITECT - RESTAURANT



ARCHITECT - MIXED-USE

**PERKINS  
+ WILL**  
LANDSCAPE ARCHITECT



CIVIL ENGINEER  
LANDSCAPE ARCHITECT



| USE                 | PARCEL 1           |                   | PARCEL 2           |                     | PARCEL 3           |             | PARCEL 4            |   | TOTAL              |                   |
|---------------------|--------------------|-------------------|--------------------|---------------------|--------------------|-------------|---------------------|---|--------------------|-------------------|
|                     | ALLOWED / REQUIRED | PROVIDED          | ALLOWED / REQUIRED | PROVIDED            | ALLOWED / REQUIRED | PROVIDED    | ALLOWED / REQUIRED  | PROVIDED  | ALLOWED / REQUIRED | PROVIDED          |
| USE                 |                    | Restaurant        |                    | Large-Format Retail |                    | Undeveloped |                     | Mixed-Use   |                    |                   |
| CURRENT ZONING      |                    | R-2               |                    | R-5-A               |                    | R-5-A       |                     | R-5-A   |                    | VARIES            |
| PROPOSED ZONING     |                    | C-2-A             |                    | C-2-A               |                    | C-2-A       |                     | C-2-A   |                    | C-2-A             |
| SITE AREA           |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| ACRES               |                    | 1.42              |                    | 5.83                |                    | 0.68        |                     | 4.04  |                    | 11.98             |
| SQUARE FEET         |                    | 61,817            |                    | 254,093             |                    | 29,721      |                     | 176,103   |                    | 521,734           |
| BUILDING FOOTPRINT  |                    | 8,400             |                    | 135,551             |                    | 0           |                     | 85,283  |                    | 229,234           |
| LOT COVERAGE        |                    | 13.6%             |                    | 53.3%               |                    | 0.0%        | 75% ††              | 48.4%   |                    | VARIES 43.9%      |
| GROSS FLOOR AREA    |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| RESIDENTIAL         |                    | 0                 |                    | 0                   |                    | 0           |                     | 278,086   |                    | 278,086           |
| NON-RESIDENTIAL     |                    | 8,400             |                    | 135,551             |                    | 0           |                     | 23,500 †††  |                    | 167,451           |
| ABOVE-GRADE PARKING |                    | 0                 |                    | 0                   |                    | 0           |                     | 62,081  |                    | 62,081            |
| TOTAL               |                    | 8,400             |                    | 135,551             |                    | 0           |                     | 363,667   |                    | 507,618           |
| FAR                 |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| RESIDENTIAL         | 3.0                | 0.00              | 3.0                | 0.00                | 3.0                | 0.00        | 3.0                 | 1.58  | 3.0                | 0.53              |
| NON-RESIDENTIAL     | 2.0                | 0.14              | 2.0                | 0.53                | 2.0                | 0.00        | 2.0                 | 0.49  | 2.0                | 0.44              |
| TOTAL               | 3.0                | 0.14              | 3.0                | 0.53                | 3.0                | 0.00        | 3.0                 | 2.07  | 3.0                | 0.97              |
| YARD SETBACKS       |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| FRONT               | 35' *              | 35'-0" MN.        | 35' *              | 35'-0" MN.          | NA                 | NA          | 35' *               | 35'-0" MN.  | VARIES             | VARIES            |
| REAR                | 15'                | 134'-7"           | 15'                | VARIES (92'-3" MN.) | NA                 | NA          | 15'                 | VARIES (23'-4" MN.)                               | VARIES             | VARIES            |
| SIDE                | 6' **              | 20'-2"            | 15'                | 21'-6"              | NA                 | NA          | 11'-3" **           | 32'-6"  | VARIES             | VARIES            |
| SIDE                | 6' **              | 80'-11"           | 15'                | 41'-8"              | NA                 | NA          | 11'-3" **           | 40'-6"  | VARIES             | VARIES            |
| COURTS              |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| EAST COURT          | NA                 | NA                | NA                 | NA                  | NA                 | NA          |                     |   |                    |                   |
| WIDTH               |                    |                   |                    |                     |                    |             | 14'-6" ***          | 58'-0"  |                    |                   |
| AREA (SQUARE FEET)  |                    |                   |                    |                     |                    |             | 450 †††             | 7,191   |                    |                   |
| WEST COURT          |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| WIDTH               |                    |                   |                    |                     |                    |             | 14'-6" ***          | 58'-0"  |                    |                   |
| AREA (SQUARE FEET)  |                    |                   |                    |                     |                    |             | 450 †††             | 10,750  |                    |                   |
| BUILDING HEIGHT     | 65'                | 23'-4"            | 65'                | 40'-10"             | 65'                | NA          | 65'                 | TOP OF ROOF: 62'-9" †<br>TOP OF PARAPET: 67'-3" † | VARIES             | VARIES            |
| PARKING             |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| RESIDENTIAL         |                    |                   |                    |                     |                    |             | 159 §§              | 183   |                    | 183               |
| NON-RESIDENTIAL     | 69 ‡               | 90 - 100          | 442 ††             | 443 §§§             | NA                 | NA          | 68 ††               | 79 ¶  | 738                | 622 §§§           |
| TOTAL               |                    |                   |                    |                     |                    |             | 227 ††              | 262 ¶   |                    | 805               |
| LOADING             |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| RESIDENTIAL         |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| LOADING BERTH       |                    |                   |                    |                     |                    |             | 1 @ 55'             | 1 @ 30' § ¶¶¶                                     | 1 @ 55'            | 1 @ 30' §         |
| LOADING PLATFORM    |                    |                   |                    |                     |                    |             | 1 @ 200 sq. ft. ¶¶¶ | 1 @ 200 sq. ft. ¶¶¶                               | 1 @ 200 sq. ft.    | 1 @ 200 sq. ft.   |
| SERVICE / DELIVERY  |                    |                   |                    |                     |                    |             | 1 @ 20'             | 1 @ 20' ¶¶¶                                       | 1 @ 20'            | 1 @ 20'           |
| NON-RESIDENTIAL     |                    |                   |                    |                     |                    |             |                     |   |                    |                   |
| LOADING BERTH       | 1 @ 30'            | 1 @ 30' *         | 2 @ 55'            | 2 @ 55'             | NA                 | NA          | 2 @ 30'             | 1 @ 30' § ¶¶¶                                     | 2 @ 55'            | 2 @ 55'           |
|                     |                    |                   | 1 @ 30'            | 1 @ 30'             |                    |             |                     |   | 4 @ 30'            | 3 @ 30' §         |
| LOADING PLATFORM    | 1 @ 100 sq. ft.    | 1 @ 100 sq. ft. * | 2 @ 200 sq. ft.    | 2 @ 200 sq. ft.     | NA                 | NA          | 2 @ 100 sq. ft.     | 1 @ 100 sq. ft. § ¶¶¶                             | 2 @ 200 sq. ft.    | 2 @ 200 sq. ft.   |
| SERVICE / DELIVERY  |                    |                   | 1 @ 100 sq. ft.    | 1 @ 100 sq. ft.     | NA                 | NA          | 1 @ 20'             | 0 @ 20' § ¶¶¶                                     | 5 @ 100 sq. ft.    | 3 @ 100 sq. ft. § |
|                     |                    |                   | 1 @ 20'            | 1 @ 20'             |                    |             |                     |   | 3 @ 20'            | 1 @ 20' §         |
| BICYCLE PARKING     |                    | 4                 |                    | 20                  |                    | NA          |                     | 60 ¶¶   |                    | 84 ¶¶             |

NOTES

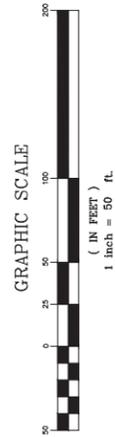
- \* Building Restriction Line per Plat (Book 199, Page 154)
- \*\* 2" per foot of height or 6' per 775.5
- \*\*\* 4" per foot of height per 776.3
- † Pursuant to 2405.3(a), applicant is seeking flexibility to increase the building height by a maximum of 5% beyond the height limit at the highest point of the parapet. Refer to sheet A03-05 for parapet elevations.
- †† With Inclusionary Zoning bonus per 2604.2
- ††† Twice the square of the minimum width or 350 square feet per 776.4
- ‡ 1 per 100 square feet in excess of 1,500 square feet
- ‡‡ 1 per 300 square feet in excess of 3,000 square feet
- ‡‡‡ Including optional mezzanine
- § Relief requested
- §§ 1 per 2 dwelling units
- §§§ 337 spaces provided on Parcel 2; 106 spaces located on Parcel 4
- ¶ Excludes 106 spaces for use by Parcel 2
- ¶¶ Within building
- ¶¶¶ Shared loading facility
- Refer to sheet C04-02 for location and configuration

**CAPITOL GATEWAY  
MARKETPLACE**

WASHINGTON, DC

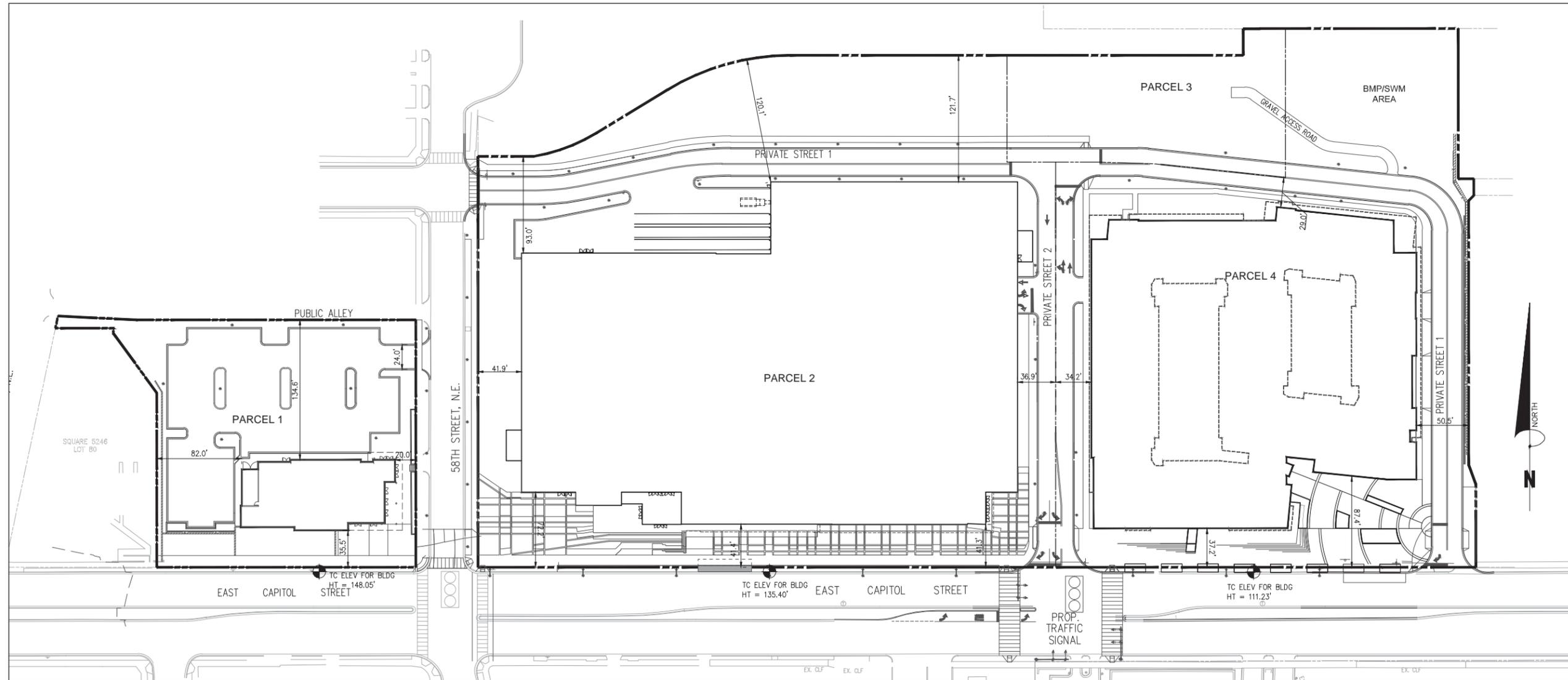
DATE  
**05.31.13**  
TITLE  
ZONING TABULATIONS

SHEET  
**C00-20**  
Page 76 of 89



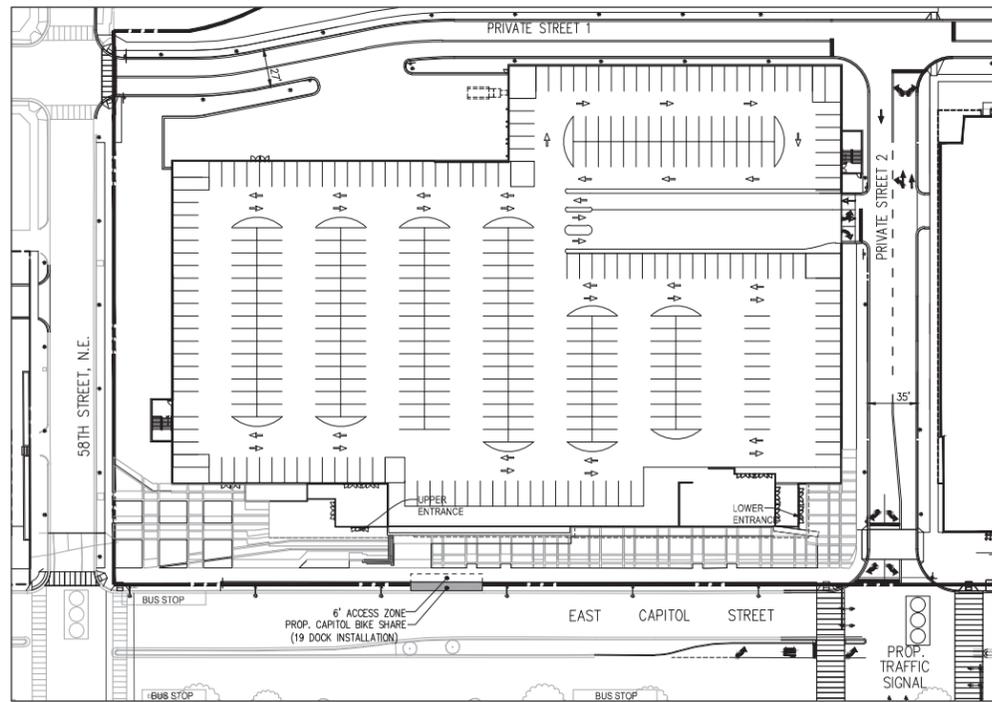
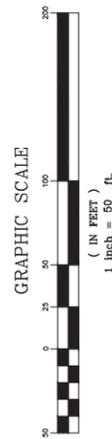
FAR TABULATION

| PARCEL       | SITE AREA (SF) | BUILDING GROSS FLOOR AREA (SF) | STREET AREA (SF) | FAR         | FAR WITHOUT STREET AREA |
|--------------|----------------|--------------------------------|------------------|-------------|-------------------------|
| 1            | 61,817         | 8,400                          | 0                | 0.14        | 0.14                    |
| 2            | 254,093        | 135,551                        | 22,069           | 0.53        | 0.58                    |
| 3            | 29,721         | 0                              | 3,846            | 0.00        | 0.00                    |
| 4            | 176,103        | 363,667                        | 25,358           | 2.07        | 2.41                    |
| <b>TOTAL</b> | <b>521,734</b> | <b>507,618</b>                 | <b>51,273</b>    | <b>0.97</b> | <b>1.08</b>             |

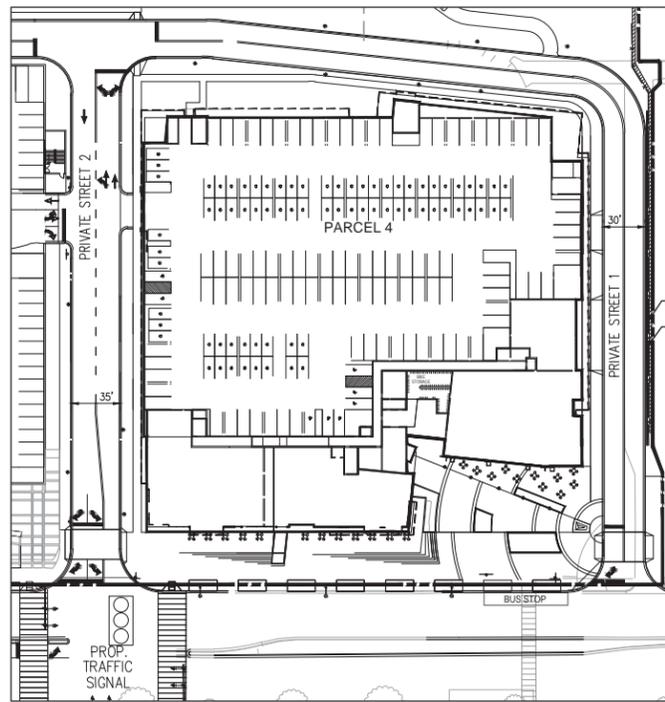


**CAPITOL GATEWAY  
 MARKETPLACE**

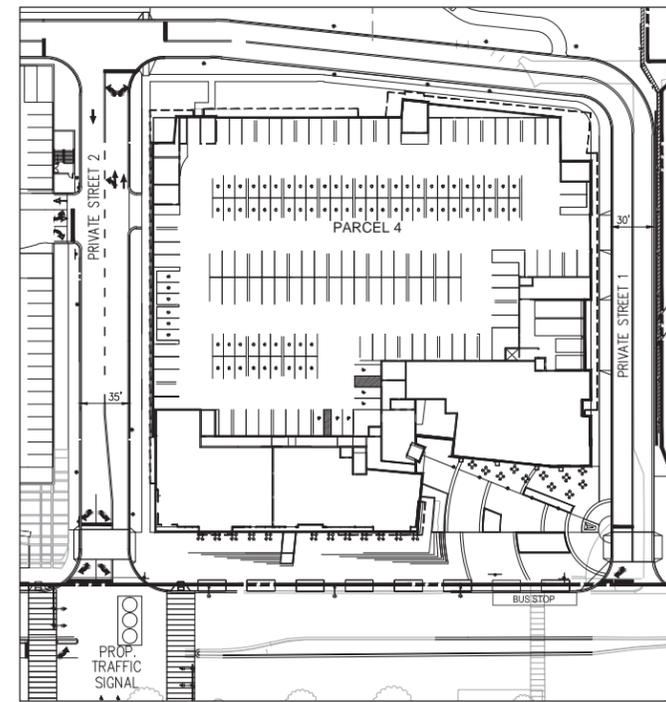
WASHINGTON, DC



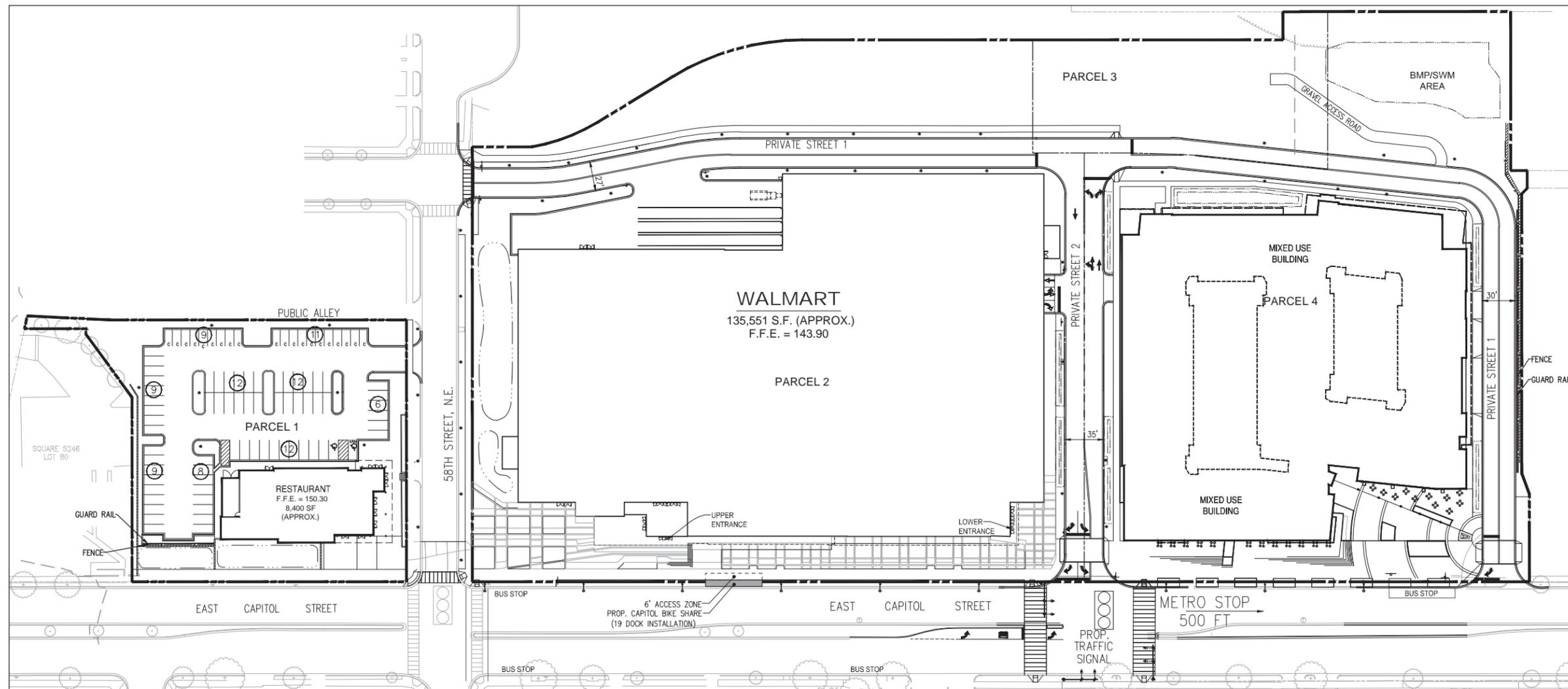
**WALMART GARAGE PARKING**  
1"=60'



**RETAIL PARKING  
LEVEL 2  
(MIXED USE BUILDING)**  
1"=60'



**RESIDENTIAL PARKING  
LEVEL 1  
(MIXED USE BUILDING)**  
1"=60'



**OVERALL SITE**  
1"=50'

APPLICANT  
A&R COMPANIE

LAND USE ATTORNEY  
Holland & Knight

ARCHITECT - WALMART  
ARCHITECT - RESTAURANT  
MMA

ARCHITECT - MIXED-USE  
PERKINS  
+ WILL  
LANDSCAPE ARCHITECT  
cgl

CIVIL ENGINEER  
LANDSCAPE ARCHITECT  
Bowman  
CONSULTING

# CAPITOL GATEWAY MARKETPLACE

WASHINGTON, DC

DATE  
05.31.13

TITLE  
OVERALL SITE  
DEVELOPMENT PLAN

SHEET



### GRADING NOTES

- CONTRACTOR IS RESPONSIBLE FOR DEMOLITION OF EXISTING STRUCTURES INCLUDING REMOVAL OF ANY EXISTING UTILITIES SERVING THE STRUCTURE. UTILITIES ARE TO BE REMOVED TO THE RIGHT-OF-WAY.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- ALL CUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE NOTED. PRECAST STRUCTURES MAY BE USED AT CONTRACTORS OPTION.
- STORM PIPE SHALL BE AS FOLLOWS UNLESS OTHERWISE NOTED: (SEE MASTER SITE SPECIFICATIONS SECTION 02630 FOR ALLOWED MATERIALS.)
- EXISTING PIPES TO BE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS.
- EXISTING GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT.
- PROPOSED GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT INTERVALS.
- IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
- ALL STORM PIPE ENTERING STRUCTURES SHALL BE GROUTED TO ASSURE CONNECTION AT STRUCTURE IS WATERTIGHT.
- ALL STORM SEWER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH PAVEMENT, AND SHALL HAVE TRAFFIC BEARING RING & COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 6" ABOVE FINISH GRADE. LIDS SHALL BE LABELED "STORM SEWER".
- THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE EPA OR APPLICABLE STATE GENERAL N.P.D.E.S. PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES AND THE WAL-MART SPECIAL CONDITIONS, SECTION 8, ENVIRONMENTAL COMPLIANCE AND STORM WATER POLLUTION PREVENTION. THIS APPLIES TO WAL-MART BUILT PROJECTS ONLY.
- CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- TOPOGRAPHIC INFORMATION IS TAKEN FROM A TOPOGRAPHIC SURVEY BY BOWMAN CONSULTING GROUP. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.
- ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3H:1V OR STEEPER. CONTRACTOR SHALL STABILIZE DISTURBED AREAS IN ACCORDANCE WITH GOVERNING SPECIFICATIONS UNTIL A HEALTHY STAND OF VEGETATION IS OBTAINED.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME.
- ALL STORM STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM INVERT IN TO INVERT OUT.

### LEGEND

- LOD — LIMITS OF DISTURBANCE
- — — — PROPOSED STORM SEWER CANOPY DRAINS
- — — — EXISTING STORM SEWER PIPING
- ⊕# PROPOSED DRAINAGE STRUCTURE INDICATOR
- ⊕# EXISTING DRAINAGE STRUCTURE INDICATOR
- ▣ EXISTING GRATED DROP INLET
- ↘ SLOPE ARROW
- ⊕ EXISTING CURB INLET
- — — — EXISTING GUIDE RAIL
- X — EXISTING CHAIN LINK FENCE
- — — — PROPOSED CONTOURS
- -232 — EXISTING CONTOURS
- - - - BREAK LINE(EXISTING BUILDING)
- ⊕ EXISTING SINGLE FIXTURE LIGHT POLE AND BASE
- ⊕ EXISTING 2 FIXTURE LIGHT POLE AND BASE
- ⊕ 2 FIXTURE LIGHT POLE AND BASE
- SPOT ELEVATION
- ⊕ EXISTING TREES

### WATER/SEWER USAGE

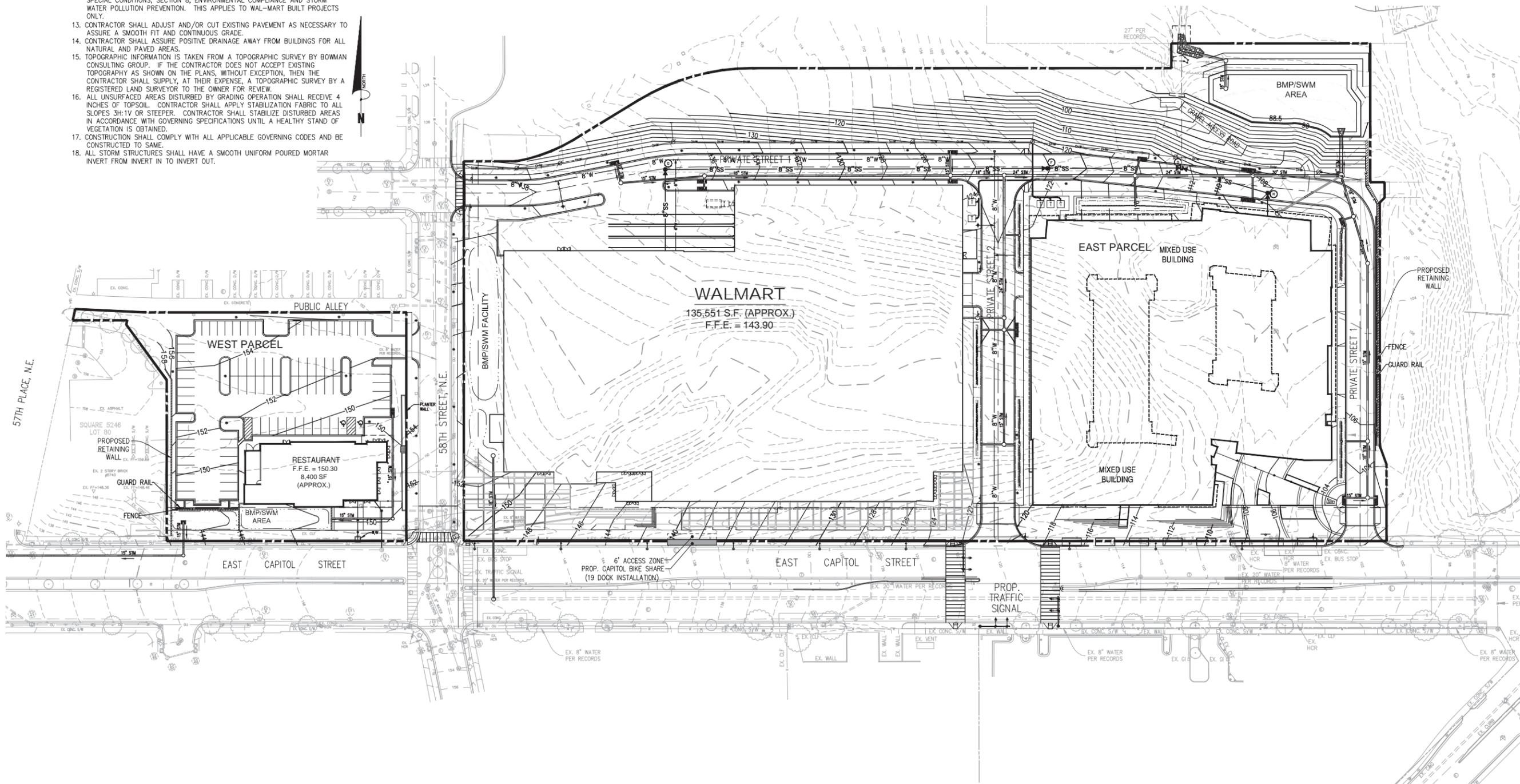
ESTIMATE 200GAL/1000SF/DAY COMMERCIAL/OFFICE/RETAIL

TOTAL COMMERCIAL/OFFICE/RETAIL:

|                            |                     |
|----------------------------|---------------------|
| WALMART                    | 135,551 S.F.        |
| MIXED USE(NON-RESIDENTIAL) | 23,500 S.F.         |
| RESTAURANT                 | 8,400 S.F.          |
| <b>TOTAL</b>               | <b>167,451 S.F.</b> |

$$\text{GALLONS} = 200 \text{ GAL} \times \frac{167,451 \text{ SF}}{1,000 \text{ SF/DAY}} = 33,490 \text{ GAL/DAY}$$

TOTAL GALLONS OF WATER/SEWER(ALL NON-RESIDENTIAL) = 33,490 GALLONS/DAY



# CAPITOL GATEWAY MARKETPLACE

WASHINGTON, DC

DATE

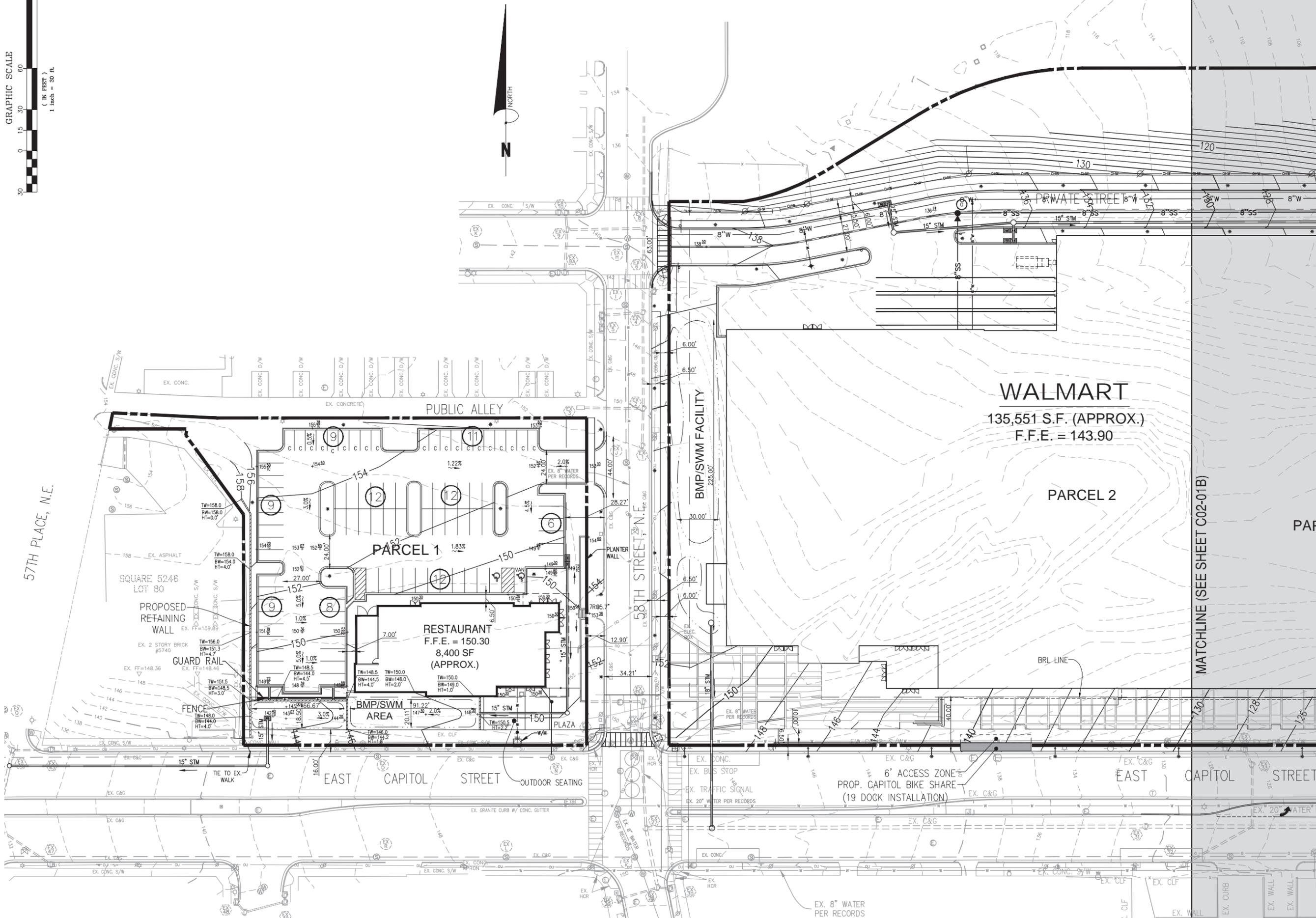
05.31.13

TITLE

OVERALL GRADING AND UTILITY PLAN

SHEET

C02-01



APPLICANT  
**A&R COMPANIES**

LAND USE ATTORNEY  
**Holland & Knight**

ARCHITECT - WALMART  
 ARCHITECT - RESTAURANT  
**MMA**

ARCHITECT - MIXED-USE  
**PERKINS + WILL**

LANDSCAPE ARCHITECT  
**cgl**

CIVIL ENGINEER  
 LANDSCAPE ARCHITECT  
**Bowman CONSULTING**

**CAPITOL GATEWAY  
 MARKETPLACE**  
 WASHINGTON, DC

DATE  
**05.31.13**

TITLE  
 PARTIAL GRADING AND  
 UTILITY PLAN - WEST

SHEET  
**C02-01A**  
 Page 80 of 89