



EAST TERRACE OFFICE BUILDING AS VIEWED FROM FIRST STREET LOOKING SOUTHWEST TOWARD THE RIVER



HALF STREET AS VIEWED FROM POTOMAC AVENUE LOOKING TOWARD THE ANACOSTIA RIVER



HOTEL AND RESIDENTIAL BUILDINGS AS SEEN FROM THE SOUTHWEST LOOKING NORTHEAST ALONG THE ESPLANADE

URBAN DESIGN STUDY AND PREHEARING SUBMISSION DRAWINGS

DAVIS BUCKLEY ARCHITECTS
AND PLANNING CONSULTANTS

100 POTOMAC AVENUE, SE
PREHEARING SUBMISSION
ZONING COMMISSION CASE NO. 01-31TE/98-17F/95-16P
APRIL 14, 2005

FLORIDA ROCK PROPERTIES, INC.
SPARKS, MARYLAND

62-21117-2-100-500
DISTRICT OF COLUMBIA
DEPARTMENT OF PLANNING
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District of Columbia
Department of Planning

CASE NO. _____
SUBMITTER NO. _____
ZONING COMMISSION
District of Columbia
CASE NO.04-14
EXHIBIT NO.16AA

100 POTOMAC AVENUE PLANNED UNIT DEVELOPMENT Urban Design Study

I. EXECUTIVE SUMMARY:

The Purpose:	This Study examines the impact of the new Washington Nationals Baseball Stadium on the context of the proposed 100 Potomac Avenue Planned Unit Development. It describes from a planning and urban design perspective how 100 Potomac Avenue, SE, will relate to and interact with the Stadium, which occupies the five city blocks directly to its north.
The Question:	Does 100 Potomac Avenue compliment the Stadium, and, if so, how will this complimentary relationship function to the benefit the surrounding neighborhoods and the District of Columbia?
The Method:	<p>This Study consisted of the following steps:</p> <ul style="list-style-type: none">• Identification of Key Considerations• Analysis of Key Considerations• Study of Physical Context (Opportunities & Constraints)• Study of Urban Design Possibilities
The Answer(s):	<ul style="list-style-type: none">• The combined 100 Potomac Avenue and Stadium developments will create a complimentary, synergistic, year-round attraction that will, together, be more than the sum of its parts.• 100 Potomac Avenue conforms to and reinforces the goals and objectives of the Anacostia Waterfront Initiative (AWI), and can help the Stadium Achieve these goals as well.• 100 Potomac Avenue compliments the stadium in massing, geometry, view and vista corridors and provision of retail venues that augment the Stadium.• The combined 100 Potomac Avenue and Stadium developments will provide retail services, entertainment venues and jobs that will revitalize the neighborhood and attract people there.

II. CONTEXT AND FRAMEWORK OF THE STUDY:

A. REVIEW OF CURRENT STUDIES IN THE AREA:

Due to impending plans to create a new Capitol Gateway by realigning the Frederick Douglass Bridge, adaptively renewing South Capitol Street, and constructing a new Major League Baseball Stadium, a broad array of urban context studies and plans have been produced by the following agencies: the Zoning Commission of the District of Columbia, the National Capital Planning Commission (NCPC), the District of Columbia Office of Planning (OP), the District of Columbia Department of Transportation (dDot), and the Anacostia Waterfront Corporation (AWC). These studies include:

1. The 2005 *"South Capitol Street"* study by the National Capital Planning Commission;
2. The January 2005 (effective date) *"Capitol Gateway Overlay"* (Order No. 971) by the Zoning Commission of the District of Columbia.
3. The October 2004 *"South Capitol Gateway Corridor and Anacostia Access Studies"* and Addendum for the District Department of Transportation (dDot) by Parsons, Brinckerhoff Quade & Douglas, Inc.;
4. The November 2003 *"South Capitol Street Corridor - Washington, D.C. - Implementation Plan"* report by the Advisory Services Panel of the Urban Land Institute;
5. The November 2003 *"Anacostia Waterfront Framework Plan"* by the District of Columbia Office of Planning;
6. The March 10, 2003 *"Zoning Commission Order No. 910-B (Florida Rock Properties PUD Extension)"* by the Zoning Commission for the District of Columbia
7. The 2001 *"Museums and Memorials Master Plan"* by the National Capital Planning Commission; and,
8. The 1996 *"Extending the Legacy"* plan by the National Capital Planning Commission

See Appendix A for excerpts from these studies.

B. DISCUSSION OF CONSIDERATIONS:

1. Considerations of Context:

a. Neighborhood Accommodation of and "Fit" of 100 Potomac Avenue and Stadium:

The District of Columbia's decision to locate the Stadium near the Anacostia River waterfront reflects an initiative, envisioned by the AWI, to jump-start the neighborhood's planned revitalization. That vision seeks make use of the presently bleak, industrial waterfront, by transforming it into a safe, environmentally friendly, economically viable, energetic, mixed-use neighborhood.

100 Potomac Avenue, which was designed in consultation with the District of Columbia's Office of Planning (OP), promotes and furthers the goals of the AWI. The Stadium also has great potential to foster and achieve these goals by making use of its extensive perimeter for year-round, ground-level, street-front retail. It will thereby avoid becoming a monolithic, single-use island, destroying linkages and deadening its neighborhood, like RFK Stadium in Capitol Hill. Abe Pollin's new MCI Center serves as a much better extant example of what a major complex, when sensitively integrated, can do to stimulate neighborhood revitalization.

b. Complementarity of 100 Potomac Avenue massing with the Stadium and Neighborhood:

The Capitol Gateway Overlay reinforced and codified the AWI vision for the Southeast and Southwest Anacostia Waterfront by emphasizing the creation of a dynamic, vibrant, mixed-use context through which a new southern gateway to the city would be built. Their vision requires that waterfront buildings be both shorter than the buildings north of Potomac Avenue, and also arranged to provide view corridors through which the northern properties can see the river.

Because of its size and its location, the Stadium could easily overwhelm any efforts to define this southern portal to the Capitol District, as viewed from across the river. 100 Potomac Avenue would provide a more gradual transition to the waterfront, softening the impact of the Stadium's height and bulk. The Stadium's estimated 130' height (this estimate excludes the lighting towers) will be measured from a significantly higher ground plane (estimated to be approximately 20 feet higher than 100 Potomac Avenue's measuring point). 100 Potomac Avenue gradually steps up from the waterfront by way of a proposed esplanade and a series of terraces. The buildings' configuration and lower height, as measured from a lower ground plane, will afford sweeping views from the Stadium to the river. The Stadium will step up farther than 100 Potomac Avenue, creating a seamless transition, a reverse-cascade from the waterfront to the crown of the Stadium as viewed from across the river.

c. Views and Vistas to and from 100 Potomac Avenue, Stadium and Surroundings:

Views from within the Stadium toward the river will be unobstructed from the skyline concourse due to the lower height of 100 Potomac Avenue's buildings. In addition, the view corridors through 100 Potomac Avenue will provide views of the waterfront and its activities both from within the Stadium and from street level.

The Stadium's walls will be visible from across the river through 100 Potomac Avenue's view corridors, while its crown will remain entirely unobstructed.

d. Compatibility of Vision and Theme:

The AWI envisions a vibrant, urban, mixed-use waterfront environment that, through linkages to adjacent neighborhoods, provides services and a lively local center of urban activity while supporting the effort to create an urban boulevard as the southern approach to the Capitol. As articulated in the various planning studies, this area is to be a rich mix of cultural, commercial and residential uses whose synergy will provide a "Place" for residents and visitors to live, work and play.

The Stadium together with 100 Potomac Avenue will create an anchor at the waterfront that will provide a year-round, 24/7, vibrant, urban environment. A Stadium, dominant in its size, will provide the local landmark that will give this portion of the waterfront its identity. 100 Potomac Avenue, while able to stand on its own, will be reinforced by the presence of this land-mark. Additionally, it will provide support to the Stadium with its extensive retail and hospitality services, ensuring the success of the Stadium as a catalyst for urban revitalization. The Stadium, a part-time venue, could not do this alone.

2. Considerations of Circulation:

a. Vehicular:

(i) Stadium Access:

While no plans for the Stadium have been proposed at this time, it can be assumed that parking access will likely be from First and/or N Streets, SE, assuming new parking is developed within the Stadium complex or located on the blocks to the north of the proposed Stadium. It is anticipated that any entrances to parking from either Potomac Avenue or P Street, SE will be minimized in order to maximize the retail frontage along whichever of these streets defines the southern edge of the Stadium .

(ii) 100 Potomac Avenue Project Access:

Vehicular Access will be offered from Potomac Avenue, which is the only city street abutting the 100 Potomac Avenue site. The ingress and egress points for both parking and loading have been minimized to two points along Potomac Avenue and should have little, if any, impact on the Stadium and its access points.

b. Pedestrian:

Pedestrian access from the Stadium to 100 Potomac Avenue and other destinations in the neighborhood, including the Waterfront, the Navy Yard Metrorail Station, the Southeast Federal Center, etc., would be accommodated via the existing network of sidewalks and via a new path, proffered as part of 100 Potomac Avenue, along the waterfront, from First Street, SE, to the Southeast Federal Center. 100 Potomac Avenue provides an important series of pedestrian linkages, from the waterfront and 100 Potomac Avenue's esplanade to Potomac Avenue and the Stadium, and to the areas to the east and south of the Stadium.

3. Considerations of Mixed-use Interdependence:

a. Mutually beneficial relationship between the Stadium and 100 Potomac Avenue:

As previously described, the Stadium - in step with the development of 100 Potomac Avenue - has the potential to help revitalize the area. The Stadium will only be used 81 days a year, and then only during the baseball season, and 11 additional days for special events. In light of this pattern of useage, it is unlikely that any retail development that is built into the Stadium plans will thrive without more critical mass with which to create a symbiotic and synergistic relationship. 100 Potomac Avenue will create this critical mass by providing destination retail, residential, hotel, and commercial development that will provide a year-round waterfront attraction.

b. Importance of density, intensity and frequency of use:

The success of a mixed-use development is dependent on three factors: density, intensity and frequency of use. A mixed-use development that is limited to one or two elements such as retail and recreational uses lacks the density of day to day users. A successful mixed-use development must have a rich mix of retail, residential and commercial uses that provides a variety of venues for dining, staying over, working and playing that is found in the most popular urban environments. The AWI seeks to create such an environment at the waterfront. The Stadium in connection with 100 Potomac Avenue and other developments in the near Southeast area and the Southeast Federal Center will provide this mix to create a vibrant area. With only the Stadium, the AWI vision and a continuous, year-round vibrant and active neighborhood with extensive array of opportunities for varied activities cannot be achieved without the critical mass of activity that will be provided by 100 Potomac Avenue.

100 Potomac Avenue provides:

- Ground-floor retail
- Commercial office space
- 325-room hotel
- 180-unit residential building
- 1,100 below-grade parking spaces

III. URBAN DESIGN STUDY FOR THE STADIUM NEIGHBORHOOD:

The following portion of this study examines the relationship of 100 Potomac Avenue to the Stadium and its surroundings in the context of the issues enumerated above. Due to the timing of the Stadium planning and design process and the absence of an actual Stadium design, certain assumptions about the likely configuration of the Stadium have been made in an attempt to identify the salient elements of the Stadium as they relate to 100 Potomac Avenue.

A. CONTEXT:

1. Vicinity Plan:

The vicinity plan (fig.1) shows the relationship between the Stadium site and the 100 Potomac Avenue site as well as elements of the surrounding "neighborhood" including the Southeast Federal Center and the Anacostia River Waterfront. It also shows the alignment of the existing Frederick Douglass Bridge and a proposed future alignment of the replacement bridge.

2. Existing Conditions:

The study area chosen for this study (fig. 1) is bounded by M Street, SE on the north; First Street, SE on the east; the Anacostia River on the south; and, South Capitol Street on the west. These boundaries generally define the immediate neighborhood of 100 Potomac Avenue and Stadium. The defined study area is seen as defining the immediate area of interaction between the two projects.

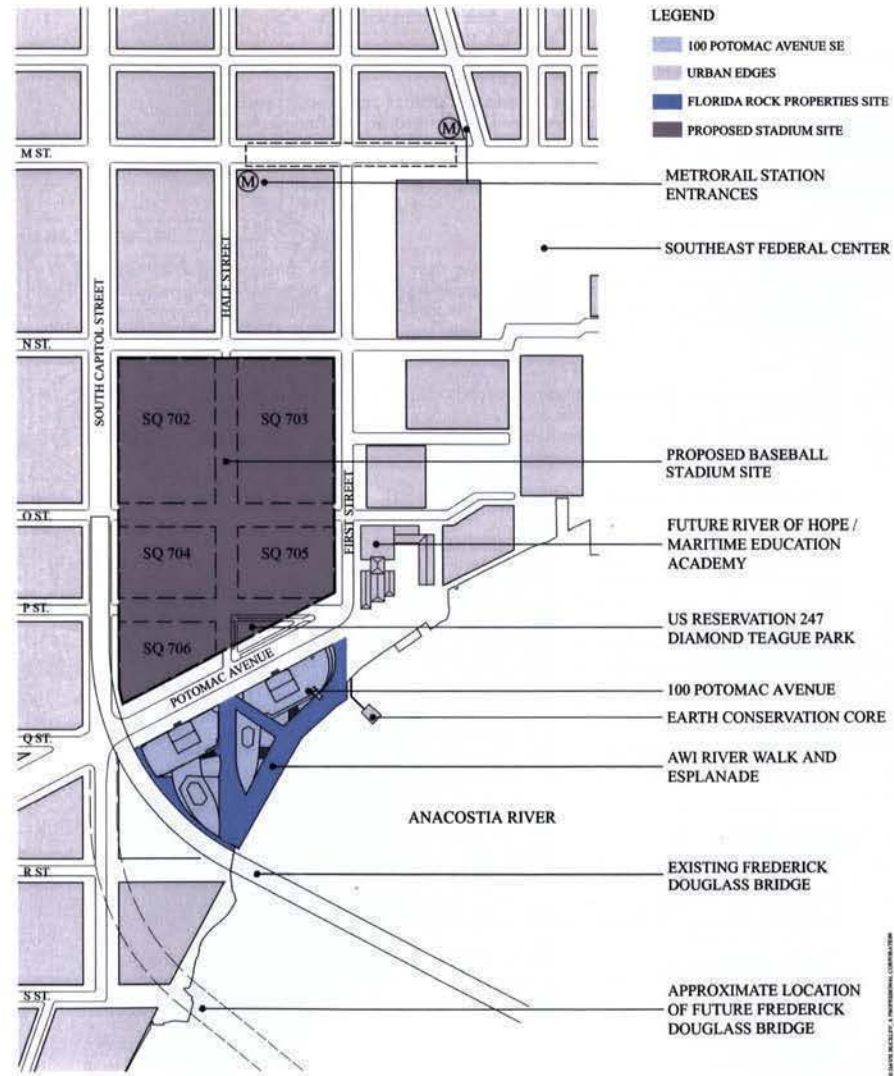
Since the Stadium is expected to demolish all existing structures on the five blocks that make up its site, the existing conditions there are of no consequence to this study.

The area to the east of the study area contains limited access and secure government facilities and the Southeast Federal Center, zoned SEFC/CR; the new Department of Transportation building, zoned C-3-C with development being governed by an approved 100 Potomac Avenue PUD, and WASA facilities, zoned CG/W-2.

To the north the Navy Yard Metro stop of the Green line has a station entrance located on the southeast corner of the intersection of M Street, SE and Half Street, SE, as well as another station entrance at the northwest corner of M street and New Jersey Avenue, SE.

To the immediate west of the study area lies South Capitol Street and the ramps to the existing Frederick Douglass Bridge. West of South Capitol Street, there are a number of small, light-industrial buildings housing an assortment of businesses. Farther west is the Carrolsburg residential neighborhood.

100 Potomac Avenue is immediately south of the Stadium site, and is currently used for industrial purposes, principally sand and gravel and concrete batching.



B. OPPORTUNITIES & CONSTRAINTS:

The Opportunities and Constraints diagram (fig.2) illustrates a series of urban design opportunities, and physical and legal constraints and other planning issues associated with the Stadium, 100 Potomac Avenue and neighboring sites including:

1. Opportunities:

a. Views to and Vistas from the Stadium:

As the Opportunities and Constraints diagram shows, there are two view corridors that run through the 100 Potomac Avenue complex: one follows the alignment of Half Street, NE extended; and, one is angled at approximately 60 degrees off the other toward the southeast, running through 100 Potomac Avenue as a pedestrian alley. The first provides a vista from the Stadium down-river with the opposite view from the water up to the south side of the Stadium. The second provides a vista from the Stadium toward the river and Anacostia beyond. Both of these view corridors open up the vista from the waterfront to the Stadium and connect the Stadium to the waterfront both visually and physically. Similarly, due to the lower massing of 100 Potomac Avenue and the higher ground at the base of the Stadium, there will be good views, from the upper level concourse of the Stadium, over 100 Potomac Avenue to the Anacostia River and beyond.

b. Stadium Fit and Iconography:

The history of Baseball Stadia gives a few examples that could serve as models for various aspects of the Stadium's design. Figures 1 and 2 show examples of how these earlier stadia were fit into their urban context in a manner that respected and maintained the "street wall" and related to the architecture of their surroundings in a complimentary way. The opportunity to recall an element of historic Baseball stadia - that has been all but lost in modern stadiums - and to create a symbolic icon for the Stadium fits well with the traditions of architecture in Washington. The precedents shown on figures 3 thru 6 show to varying degrees the incorporation of a tower element that becomes a symbol for the Stadium it graces.

A potentially effective way to define the portal would be to mark the edge of the portal with an iconic architectural element that could also be a symbol for the Stadium. This potential iconic element could be located on Square 706, adjacent the South Capitol Street/Potomac Avenue intersection, framing the view of the Capitol and providing a "symbol" for the Stadium.

Such a element could be developed, in a contemporary idiom, that could serve, not only as a symbol for the Stadium, but as a vertical circulation tower for the upper areas of the Stadium, and perhaps as an observation tower. Its location could be easily integrated with a drop-off for disabled and/or VIP patrons of the stadium (see suggested circulation flow on Opportunities & Constraints diagram, fig. 2)

c. Synergy between 100 Potomac Avenue and Stadium:

The simultaneous completion of 100 Potomac Avenue and Stadium projects will foster a symbiotic synergy between the two whereby patrons of the Stadium can avail themselves of 100 Potomac Avenue's dining and other retail venues and tenants and residents of 100 Potomac Avenue can use the Stadium's facilities. As shown on the Opportunities and Constraints diagram (fig. 2), there are multiple sites in 100 Potomac Avenue for outdoor dining facilities as well as retail opportunities in the alley and other ground floor retail venues possible in 100 Potomac Avenue. It should also be possible to develop ground level retail

venues under the Stadium seating tiers. The provision of this additional ground floor retail as part of the Stadium in conjunction with the retail of 100 Potomac Avenue will assure the creation of a "critical mass" of destination oriented retail at the Stadium neighborhood, especially along Potomac Avenue.

d. Creation of a mixed-use destination retail that serves neighborhood as well as Stadium:

The location of the proposed Stadium and 100 Potomac Avenue will serve as a catalyst for the redevelopment of the Anacostia waterfront as well as the nearby Southwest residential neighborhood to the west by providing a complimentary assortment of services and attractions. This draw from the local neighborhood can also help to assure the viability of any retail associated with the Stadium and 100 Potomac Avenue, particularly when the Stadium is not in use.

2. Constraints:

a. Zoning Regulations:

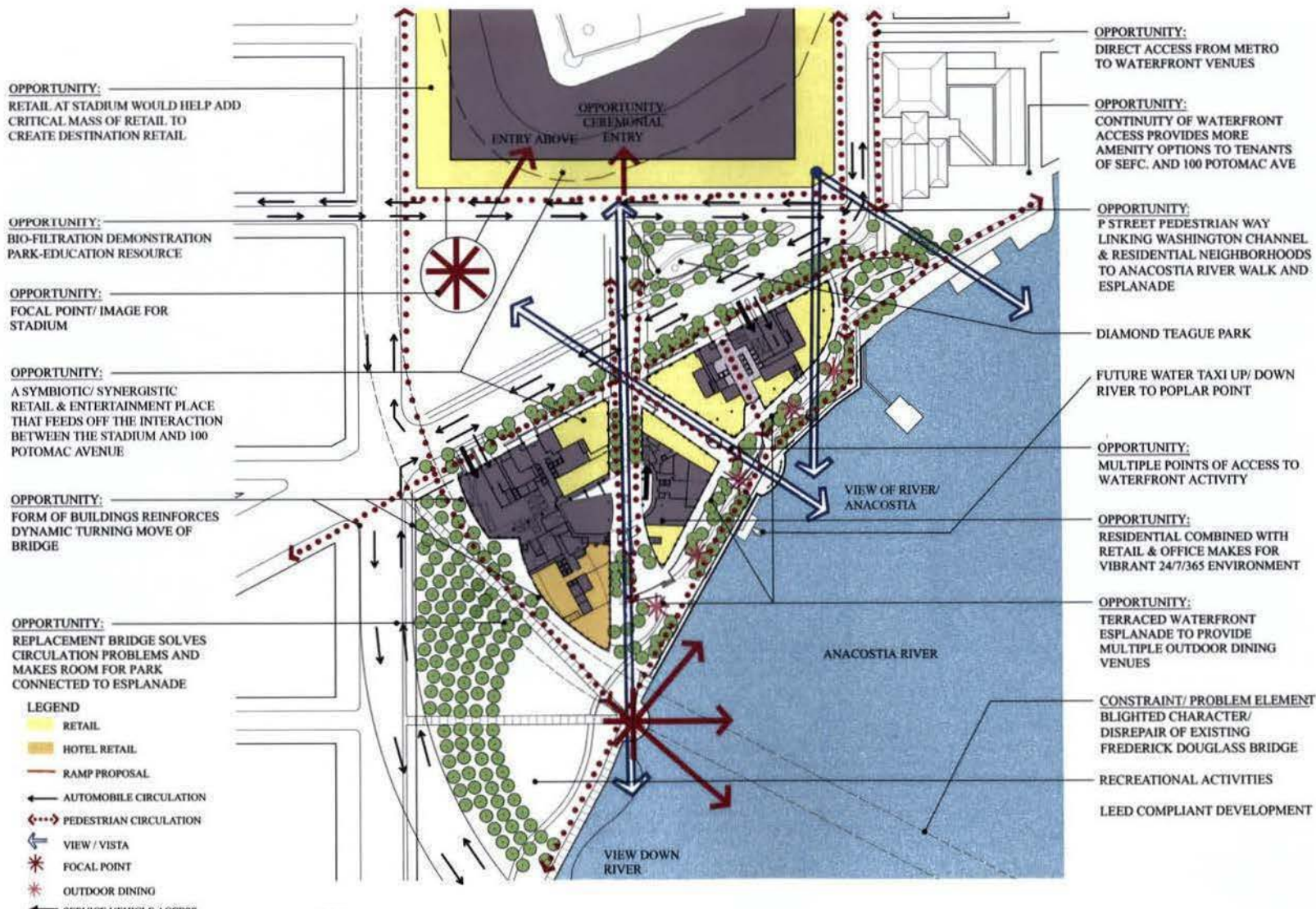
The operative zoning regulations with relevance to the Stadium include the CR and W-2 sections of the Zoning Regulations along with the CG (Capital Gateway) Overlay and proposed text amendment to the CG Overlay. 100 Potomac Avenue is governed by the C-3-C District, to be developed in accordance with the approved 100 Potomac Avenue. The following constraints on the uses and height, bulk and mass of developments in these areas is summarized as follows:

<u>Development Entity:</u>	<u>Height Limitation</u>	<u>Floor Area Ratio (FAR) Limitation</u>
Stadium*	130' (Height Act of 1910) + any portion of Stadium that exceeds 110 feet in height shall be stepped back at 1:1 on South Capitol Street.	6.0 far; however, per Section1606.4 of the proposed text amendment to the CG Overlay, no portion of the FAR need be used for residential purposes within the Ballpark Area.
100 Potomac Avenue PUD	110' for the West Office Building and Hotel;100' for the Residential Building; and, 90' for the East Office Building per the approved Design Guidelines for the PUD	4.4 per Design Guidelines by O.P.

* per the March 4,2005 "Ballpark" amendment to the CG Overlay proposed to the Zoning Commission.

b. Existing Frederick Douglass Bridge:

The existing Frederick Douglass Bridge (the "Bridge") represents a constraint on the ideal redevelopment of the Stadium Neighborhood on several counts: (1) the Bridge is in deteriorated condition and as such represents an eyesore; (2) the Bridge occupies a prime location on the Anacostia waterfront that effectively breaks the continuity of the esplanade development proposed for 100 Potomac Avenue and acts as a barrier to interaction between 100 Potomac Avenue and any future green space to the southwest of the existing bridge; and (3) the current configuration of approach ramps, starting at O Street, SE, because it is elevated, is not conducive to creating an appropriate streetscape for a principal approach to the city. This issue is being addressed by dDot and is not anticipated to impact the complementarity of the Stadium/100 Potomac Avenue combination.



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OPPORTUNITIES & CONSTRAINTS

ZONING COMMISSION CASE NO. 01-311E/99-17/95-14P
STADIUM & 100 POTOMAC AVENUE PUD
URBAN DESIGN STUDY

PRE-HEARING SUBMISSION
APRIL 14, 2005

FIG 2

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1 SHIBE PARK / "CONNIE MACK" STADIUM, PHILADELPHIA, PENNSYLVANIA, 1909-1970 - STADIUM FITS INTO AN ACTIVE URBAN SETTING



2 EBBETS FIELD, BROOKLYN, NEW YORK, 1913 - ANOTHER STADIUM THAT "FITS" INTO THE URBAN STREETSCAPE



3 ORIOLE PARK AT CAMDEN YARDS, BALTIMORE, MARYLAND, 1992 - STADIUM MAKES URBAN ARCHITECTURAL GESTURES AND CREATES SPECIAL ENTRY PLAZA IN "SYNCH" WITH GEOMETRY OF SEATING TIERS, UPPER CONCOURSE AND SETBACK



4 COORS FIELD, DENVER, COLORADO, 1993 - STADIUM ENTRY REINFORCES GEOMETRY OF SEATING TIERS AND CREATES SYMBOLIC IMAGE FOR THE STADIUM



5 SHIBE PARK / "CONNIE MACK" STADIUM, PHILADELPHIA, PENNSYLVANIA, 1910-1970 - AN EXAMPLE OF AN ICONIC TOWER PLACED WITHIN THE GEOMETRY OF STADIUM



6 WRIGLEY FIELD, LOS ANGELES, CALIFORNIA, 1925-1961 - ANOTHER EXAMPLE OF AN ICONIC TOWER PLACED IN AN ASYMMETRICAL COMPOSITION, REINFORCES GEOMETRY OF ENTRANCE

STADIUM SKYLINE CONCOURSE

SYMBOLIC / CULTURAL / ICONOGRAPHIC

The element identified on the Geometry/ Massing diagram as an Iconic Element represents the architectural opportunity to establish an Iconic image for the Stadium. As illustrated above under Opportunities and Constraints, there are numerous examples of Baseball Stadia that use a tower or other iconic element to establish an image for the Stadium. The vista toward the Stadium along the axis of the pedestrian allee of the PUD, extended to intersect with the centerline of the stadium seating geometry, suggests a place to locate such an iconic element that could not only serve a circulation and sightseeing function but provide a strong and dynamic image for the Stadium as well. Such an element, allowed as an "architectural embellishment" by the zoning ordinance could have visibility form across the river and frame the vista of the Capitol from the intersection of the new bridge with Potomac Avenue.

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ICONOGRAPHY OF STADIA
ZONING COMMISSION CASE NO. 01-317E/98-177/95-14P
STADIUM & 100 POTOMAC AVENUE PUD
URBAN DESIGN STUDY

PRE-HEARING SUBMISSION
APRIL 14, 2003

FIG 3

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C. DESIGN CONCEPTS:

1. Geometry / Circulation Diagrams:

The massing diagram (fig. 3) shows a possible configuration for the Stadium geometry that follows recommended practice for Stadium planning in its orientation of the seating wings and the playing field. The diagram illustrates how the geometry and massing of 100 Potomac Avenue relates to this possible Stadium geometry and how the combination of the two seizes upon some of the vista opportunities described in the "Opportunities, Issues and Constraints" section of this study.

2. Circulation:

The circulation for the Stadium and 100 Potomac Avenue Project would be the same as shown on the opportunities and constraints diagram as follows:

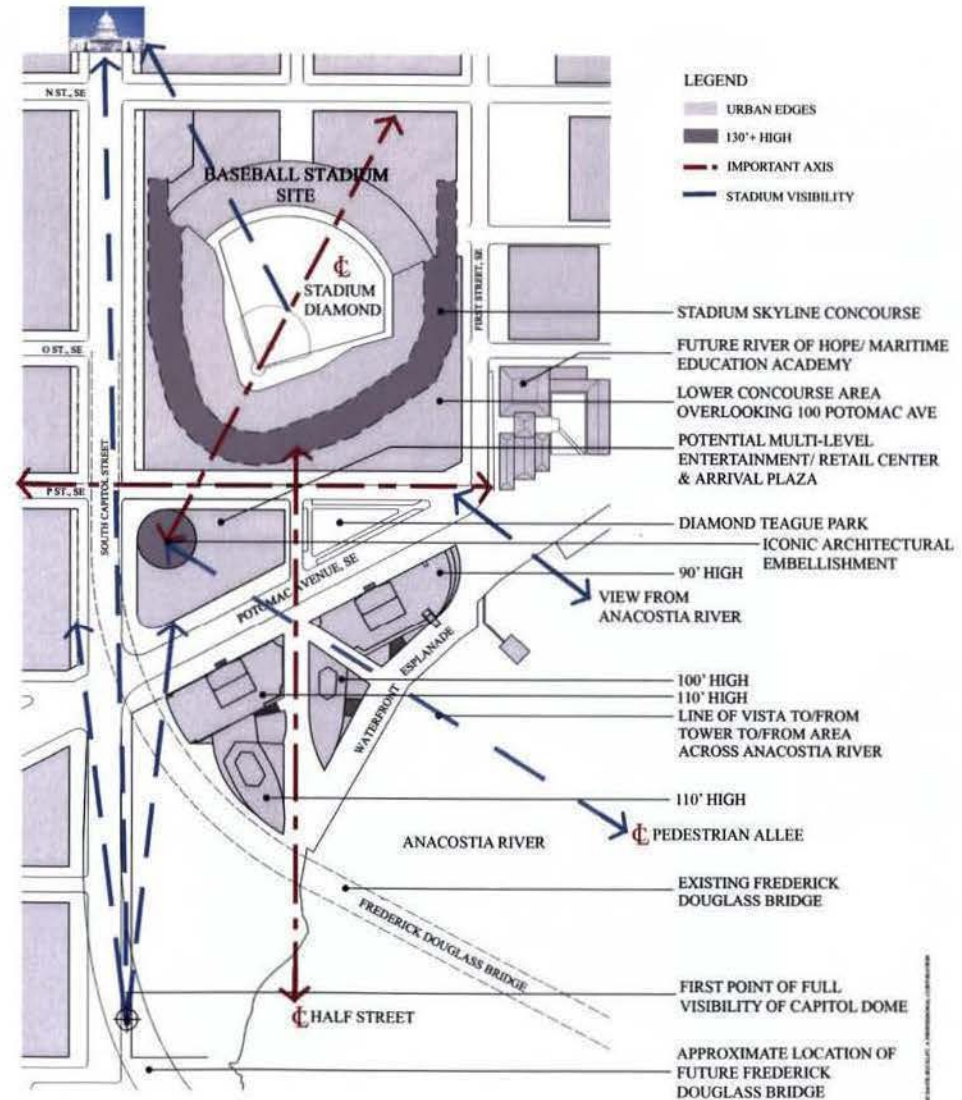
a. Vehicular Circulation:

Automobiles will utilize Potomac Avenue and First Street, SE for access to both 100 Potomac Avenue and the Stadium. Parking access to 100 Potomac Avenue is proposed off Potomac Avenue as the only city street abutting the Project. It is anticipated that a significant number of spaces will be available in the below-grade parking area for Stadium use, especially on evenings and weekends. It is anticipated that the automobile access for the parking under or adjacent the Stadium will occur from First Street, and M and N Street SE, with Potomac Avenue providing a queuing area for cars coming to the Stadium that are approaching from the Potomac / South Capitol Intersection. The triangular park provides an opportunity to create a vehicular drop-off loop off Potomac Avenue that has as its centerpiece an environmentally friendly park at Reservation 247 proposed as part of 100 Potomac Avenue.

Access to the Stadium by car would not be significantly impacted by 100 Potomac Avenue, excepting that there will likely be a number of parking spaces available to the Stadium patrons for day games and more available for night games when the office tenants who are not attending the game go home. As is shown on the circulation diagram, all vehicular access to 100 Potomac Avenue will be via Potomac Avenue. Service vehicles access 100 Potomac Avenue off Potomac Avenue. It is anticipated that service vehicles will access the Stadium from First Street and N Street, SE; and, in accordance with the proposed text amendment to the Capital Gateway Overlay requirements, will occur within the Stadium structure.

b. Pedestrian Circulation:

100 Potomac Avenue was designed as a very pedestrian-oriented complex, even before the advent of the Stadium. As such, this heavily retail-oriented, outdoor streetscape will be a major attraction, for both pre-game and post-game, as well as off season activities such as concerts, with shops and restaurants that encourage strolling and window shopping. Strong pedestrian linkages to the Stadium will occur at each of the three major intersections, Potomac Avenue and South Capitol, Half, and First Streets, SE. Each of these lead from the Stadium to the major gateways to 100 Potomac Avenue's ground level retail areas at the intersections of Potomac Avenue and First, Half and South Capitol Streets, SE.



IV. CONCLUSION:

The AWI contemplates an active, vibrant, mixed-use waterfront that will serve as a stimulus to help revitalize the adjacent neighborhood(s). 100 Potomac Avenue promotes and furthers the AWI goals for the Anacostia River waterfront and will serve as a catalyst for future development along the waterfront to enable the visions of the AWI to be fulfilled. The advent of the new Washington Nationals Baseball Stadium in the vicinity of the waterfront inserts a new and significant element into the milieu. The Stadium has the potential to ensure that the AWI vision is achieved, if it is developed in accordance with the AWI's principles.

It is the conclusion of this study that the Stadium and 100 Potomac Avenue are compatible and will further the goals for this area for the following reasons:

- All of the issues that precipitated the AWI are still present in the neighborhood. 100 Potomac Avenue is entirely consistent with the goals of the AWI because it provides all of the elements of the desired mix. Further, it contains the characteristics of successful urban environments: density, intensity and frequency of use.
- The Stadium can and should be incorporated into the existing urban fabric rather than resorting to the failed planning approaches that produced the Stadium's predecessor, RFK Stadium.
- 100 Potomac Avenue will help to reduce the apparent mass of the Stadium and focus the views from the waterfront to the Stadium, including, as is indicated on the Opportunities and Constraints diagram, a potential focal, iconic tower.
- 100 Potomac Avenue represents a plan that supports the goal of promoting a mixed-use development on the Anacostia waterfront. The mix of office, residential, hotel and retail at 100 Potomac Avenue will assure a vibrant, active, "street life" in the neighborhood. 100 Potomac Avenue also furthers the goal of providing inter-connectivity between various areas surrounding it, including an important visual and physical connection between the waterfront and the Stadium.
- 100 Potomac Avenue provides an exceptional amount of open space through setbacks, the waterfront esplanade, the Half Street view corridor, the pedestrian alley, the development of parklike settings to the east of the site and on Federal Reservation 247, and the reservation to the land beneath the current Bridge right-of-way for green, open space. Accordingly, 100 Potomac Avenue makes a major contribution to establishing the open space character of the area.
- 100 Potomac Avenue is consistent with the *South Capitol Study's* version of development on the 100 Potomac Avenue site with the exception that 100 Potomac Avenue includes more office and retail space, as well as residential space to round out a truly mixed-use development that would be essential to the viability and vitality of the Stadium and the re-development of the area. 100 Potomac Avenue, when combined with the Stadium and its potential retail component, would go a long way toward establishing a "critical mass" required for a successful retail environment.
- The combination of the Stadium with a ground floor retail component and 100 Potomac Avenue's mix of urban amenities will create a symbiotic critical mass that will serve the neighborhood and the District through a synergy that will be greater than the sum of its parts.



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ARCHITECTS & PLANNERS



URBAN DESIGN - ILLUSTRATIVE SITE PLAN
ZONING COMMISSION CASE NO. 01-317E04 (17/95-14P)
STADIUM & 100 POTOMAC AVENUE PUD
URBAN DESIGN STUDY



PRE-DEVELOPMENT SUBMISSION
APRIL 14, 2001

FIG 5

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APPENDIX A: Excerpts from Previous Studies in the Capitol Gateway Area

A review of the available material from these recently completed and, in some cases, in-progress studies yields a number of important policy positions that have relevance to and have provided guidance for the Florida Rock 100 Potomac Avenue proposal. These points are noted below under the heading of the document from which they were extracted:

“South Capitol Street”:

- “...NCPC has had its sights on a long-range vision for this neglected area - a vision to transform the avenue into a boulevard with magnificent spaces for public plazas, parkland, national monuments and commercial and residential uses. NCPC first proposed this concept in the agency’s 1997 *Legacy Plan*.”
- “Anacostia Waterfront would become a mixed-use ... area.”
- “South Capitol Street/South of M Street would have a larger scale with longer blocks, fewer street crossings and a green median to connect with the river.”

“Capitol Gateway Overlay”:

- Zoning Commission expressed “concern for the creation of an active pedestrian streetscape.”
- “The CG Overlay will create an opportunity for an active mixed-use community in the Buzzard Point-South Capitol Street area.”
- Purposes of the CG Overlay District are to:
 - (a) “Assure development of the area with a mixture of residential and commercial uses, and a suitable height, bulk and design of buildings, as generally indicated in the Comprehensive Plan...”
 - (b) “Encourage a variety of support and visitor related uses such as retail service entertainment, cultural and hotel or inn uses.”
 - (c) “Allow for continuation of existing industrial uses, which are important economic assets to the city, during the extended period projected for redevelopment.”
 - (d) “Provide for a reduced height and bulk of buildings along the Anacostia Waterfront in the interest of ensuring views over and around waterfront buildings, and provide for continuous open space along the waterfront with frequent public access points.”

“South Capitol Gateway Corridor and Anacostia Access Studies”(Including the ‘Ballpark Addendum’:

- Study “Performed at the direction of the United States Congress...was ‘a study of methods to make improvements to promote commercial, recreational, and residential activities and to improve pedestrian and vehicular access on South Capitol Street and the Frederick Douglass Memorial Bridge...’.” also included the Anacostia Access Study.
- “...encourage mixed-use development...”
- “The addition of the ballpark to the mix does not significantly affect the corridor concept or benefits because the previously conceived plan for the Gateway anticipated development of the scale of the Ballpark.”

“South Capitol Street Corridor, Washington, DC”:

- The study identified the “Anacostia Waterfront as an east-west connector and activity magnet.”
- The study recommended that the City “Create economic connections between existing and potential residents and commercial tenants and the developers who serve them.”

- “Estimated capture rates for the South Capitol Study area suggest that demand for both residential and office space in the district may meet or exceed estimated supply over the next 20 year period.”
- With respect to the quadrant southeast of M and South Capitol: “The area south of M Street to the Anacostia River has already been established as predominantly a market for office and mixed-use development.”
- “...the office space available for development along the South Capitol Street Corridor will barely be sufficient.” (To meet projected demand)
- With respect to market demand: “It seems likely that a more substantial, mixed-use urban development program with street level retail and above-grade residential and office can be achieved.”
- With respect to the Frederick Douglass Memorial Bridge viaduct: Altering the viaducts to land at Potomac Avenue instead of almost at the intersection of N Street will:
 - (a) Double the at-grade length of South Capitol Street, creating more available street frontage for ground level retail or commercial development, while stitching the neighborhood together into a cohesive whole; and
 - (b) Initiate the process of street improvement potentially as much as 12 years before it otherwise could begin (i.e. before the new bridge is in place).
- With respect to the Florida Rock (100 Potomac Avenue PUD) site: “The continuation of Anacostia Riverside Park is envisioned as a 75- to 100-foot wide public right-of-way along the north bank of the river to Buzzard’s Point...Larger parks could be located adjacent to the Riverside Park (adjacent to the Florida Rock Site) south of Potomac Avenue.”

“The Anacostia Waterfront Framework Plan”

- “If this development is guided by a comprehensive vision, the infusion of billions of public and private dollars can transform a once industrial area into a vibrant waterfront neighborhood.”
- “Planning Principles (for the Near Southeast Waterfront):
 1. Extend the surrounding urban fabric to the waterfront, bringing the city to the Anacostia River.
 2. Build upon the current wave of public and private development to create a comprehensive vision for the Near Southeast, integrating diverse projects.
 3. Create continuous public access to the Anacostia River waterfront, as part of the Anacostia Riverwalk and Trail.
 4. Create a linked network of public parks, open spaces, greenways, and tree-lined streets to tie the Near Southeast neighborhood together and to the waterfront.
 5. Create a major waterfront park destination for residents, employees, and visitors.
 6. Maximize access to the waterfront from residential areas by extending existing streets and view corridors to the river.
 7. Emphasize mixed-use development, integrating commercial and residential areas, to form a lively and active neighborhood throughout the Near Southeast.
 8. Provide diversity in housing types and income levels to ensure a strong and balanced neighborhood.
 9. Encourage commercial development to maximize economic growth and job creation, emphasizing major street corridors and transit connections.
 10. Create linkages to the adjacent neighborhoods of Capitol Hill and Southwest by overcoming the physical and psychological barriers of the highway network.
 11. Encourage low-impact development with “green” building techniques for sustainable architecture and landscape design.”
- “Vision (for the South Capitol Street Corridor): This area will become a mixed-use employment corridor and a significant gateway to the Capitol...”

“Zoning Commission Order No. 910-B (Florida Rock Properties PUD Extension)”

- Attachment to Z.C. Order No 910-B “Design Guidelines for Modification of Planned Unit Development, Florida Rock Properties, Inc (“FRP”), Zoning Commission Case No.: 01-31TE/98-17F”:

I. 100 Potomac Avenue

1. Site Plan Organization

- a. The site plan for the PUD Site shall be modified to reflect the proposed development of a minimum of three (3) independent buildings, creating a minimum of two (2) publicly-accessible, primarily pedestrian oriented passages through the PUD site for access from Potomac Avenue to the Anacostia River waterfront (the “Waterfront”).

2. Site Perimeter Setback and Build-top Requirements

- a. Potomac Avenue, SE - Buildings fronting along Potomac Avenue, SE shall be designed to face on the Potomac Avenue, SE right-of-way of line with no setback from that right-of-way, except for facade articulation, and fenestration and breaks for pedestrian access to the Waterfront.
- b. First Street, SE - No building, fronting on what would be a theoretical extension of the right-of-way of First Street, SE through the PUD Site to the Waterfront, shall extend into area of the PUD site covered by this theoretical extension of the First Street, SE right-of-way.
- c. Anacostia River - To provide space for a broad esplanade for the full length of the PUD site along the Waterfront, buildings would be set back at least seventy-five (75) feet from the exterior face of the Anacostia River bulkhead along the PUD site. This setback line shall be perpendicular to the Anacostia River Bulkhead. The design of the facades of buildings fronting on the Waterfront shall be further modulated behind this setback line to achieve a variety of setback dimensions for the buildings fronting on the Waterfront.
- d. Frederick Douglass Bridge - All buildings on the PUD Site shall be set back from the eastern edge of the structure of the bridge. Buildings shall not be located closer to the structure of the Bridge than the eastern boundary of the established right - of - way fixed in the official records of the District of Columbia, within which the bridge structure is located. Actual building site locations fronting on the Bridge shall be coordinated with and may be adjusted based upon the findings of the DC DOT Corridor Study for the South Capitol Street and Bridge relocation being undertaken as of the date of these Design Guidelines (“DC DOT Corridor Study”).

3. Mid-block Points of Public Access to the Waterfront

- a. Half Street, SE - The development plan for the PUD Site shall include the theoretical extension of Half Street, SE to the Waterfront as a pedestrian-focused, open-to-the sky, publicly-accessible passageway, with a width of no less than sixty (60) feet at any point along the passageway, and with a width of no less than sixty (60) feet at any point along the passageway; a limited number of motorized vehicles may be permitted to use the passageway to permit vehicular access to the proposed residential development, including a possible hotel fronting on the Waterfront.
- b. Additional Access Through the PUD Site - The development plan for the PUD Site shall include a minimum of one additional pedestrian-oriented, open-to-the sky, publicly-accessible passageway, with a width of no less than forty (40) feet at any point along the passageway; the passageway shall be located east of Half Street, SE, and west of First Street, SE, with this passageway having the intended purpose of providing an additional pedestrian-oriented passageway from Potomac Avenue, SE to the Waterfront.

4. Building Height, Bulk Restrictions, and Design Objectives

- a. General - Height of buildings on the PUD Site shall create a varied silhouette of building

heights, as seen from across the Anacostia River. With that in mind, any building(s) located in the area of the PUD Site west of the theoretical extension of Half Street, SE (as described in Item 3 above), the height of building may not exceed 110 feet; for the area east of the theoretical extension of Half Street, SE and west of the additional access through the PUD Site (as described in Item 3.b. above), the height of any building shall not exceed 100 feet, provided that OP and FRP may explore an increase in height of any building proposed to be located in this area if the same would increase the amount of non-transient residential housing in the PUD; and for the area east of the additional pedestrian passageway described above and First Street, SE, the height of building may not exceed ninety (90) feet.

- b. Potomac Avenue, SE - The building(s) fronting on Potomac Avenue, SE shall rise to allowable heights with no setbacks in the massing. Building facades shall be developed so as to create a street-wall condition, which engages the historic L’Enfant grid, provided that facade articulation, fenestration, and possible setbacks of the building facades at upper elevations of the buildings shall be permitted.
- c. First Street, SE - At a minimum, the buildings fronting on First Street, SE (including the theoretical extension thereof) shall setback a minimum of ten (10) feet above the height of sixty-five (65) feet. Primary building material may be glass.
- d. Anacostia River Waterfront - The buildings fronting on the Waterfront shall be articulated with varying setbacks of different widths and dimensions at various elevations along the Waterfront facing facades to avoid a monolithic appearance for the buildings along the Waterfront, the intent being to create a multifaceted and interesting project appearance along the Anacostia River, coordinated with the various vistas, views, passageways, and open spaces on the PUD Site to be developed with any application for modification of the PUD as approved.
- e. Frederick Douglass Bridge Facade - The facade of buildings fronting the Bridge and its right-of-way will reflect this area as a major gateway to the monumental core of Washington, D.C. at the foot of South Capitol Street; facade development will also be evaluated within the recommendations of the DC DOT Corridor Study.
- f. Facade Materials of PUD Buildings - Building materials shall be primarily masonry and glass in character. Variation in materials colors shall distinguish the buildings on the PUD Site from one another so as to create an ensemble of buildings rather than the appearance of a single large structure.

5. Development Program Requirements

- a. General - The intention of the development program is to create a mixed-use, waterfront environment of residential, office, and commercial uses.
- b. The ground levels of all buildings shall maximize uses, which open to and are intended to activate the adjacent streets and planned open spaces. A minimum of fifty percent (50%) of the net useable area of the aggregate of all ground levels in the PUD shall be designated to retail, cultural, or community uses, which uses it is believed will serve to promote and encourage visitation of the Waterfront. All ground level areas shall be designed to allow a twelve (14) foot floor-to-ceiling height and shall be designed so as to anticipate future changes in program use. Ground level areas fronting on the Waterfront should be designed to give the appearance from the esplanade that those ground level spaces are multi-storied, spacious and open. Understanding that there is an elevation change in the PUD site from Potomac Avenue, SE down to the Anacostia River, net useable areas of the various ground levels of the buildings shall be deemed to be those areas of the ground levels that directly front on Potomac Avenue, SE, First Street, SE, the passageway described in item 3 above,

and the Waterfront, and which would be reasonably accessible from those areas and useable for the purposes described above; the term "useable area" shall specifically exclude areas on those levels designated or used for building entrances and lobbies and related facilities, elevator banks, staircases and corridors related thereto, mechanical, electrical and fire control rooms, parking garage entrances and other related facilities, off-street loading facilities and other related facilities, and service corridors related to any of the above.

- c. Those portions of the ground levels fronting on the Bridge in buildings will be evaluated in light of the recommendations of the DC DOT Corridor Study to determine if a more animated ground level area in that location would be appropriate to ensure that these areas can appropriately address possible pedestrian presence in those locations.
- d. All legally required, on-site parking shall be located below grade; other parking provided may be located above grade, but shall be located so as not to impede pedestrian uses of the open spaces, vistas and views on the PUD Site or prevent the dedication of ground level spaces to preferred uses as specified in paragraph 5.b. above.
- e. The remaining development program above ground levels shall include commercial and residential uses (including potential hotel uses) with a maximum allowable commercial development potential of 625,000 gross square feet; and a minimum residential development of 440,000 square feet of gross floor area of hotel and residential uses, provided that no less than 160 units of residential, non-transient housing, based upon an average gross floor area of 1,200 square feet per unit would be provided for.
- f. The maximum permitted building area on the PUD Site shall be 1,115,400 gross square feet for a total of 4.4 FAR

6. Project Amenities of PUD

- a. General - The general approach to the PUD amenities shall consist of public space improvements in and about the vicinity of the PUD Site, including areas to the north and east of the PUD Site.
- b. Public Access to the Waterfront - Access through the PUD Site to the Waterfront, including plaza connections from Potomac Avenue, SE, shall be maintained as privately-owned, publicly-accessible, and appropriately landscaped open spaces.
- c. Anacostia Esplanade and Riverwalk - The PUD shall provide for continuous publicly-accessible esplanade of no less than seventy-five (75) feet in width, on the PUD Site, including designated walkways and bicycle lanes. In addition to development of the esplanade on the PUD Site, FRP would design and develop a riverwalk/pathway of no less than twelve (12) feet in width stretching eastward from the PUD Site to the site known as the Southeast Federal Center ("SEFC"), over sites owned by the District of Columbia and the DC Water and Sewer Authority. The riverwalk/pathway would be intended to afford a pedestrian and bicycle connection between the esplanade on the PUD Site and the proposed SEFC riverside facilities. Waterfront redevelopment is not contemplated as being part of this amenity. FRP would maintain the riverwalk/pathway for a period of no less than five (5) years after its development.
- d. Parks and Plazas - In addition to the esplanade and open spaces on the PUD Site and the riverwalk/pathway described above, FRP shall design and develop public open spaces at two locations adjacent to the PUD Site as urban parks. The first space would be the triangle park reservation to the north of the FRP site on Potomac Avenue ("Reservation 247"), containing approximately 16,000 square feet of land area more or less; the second space would be an area at the terminus of First Street, SE immediately adjacent to the PUD Site and fronting on the ECC pump house. FRP shall maintain each of these public spaces for a period of no less than five (5) years after its development.

- e. Sustainable Design - All buildings on the PUD Site shall be designed to achieve USGBC LEED certification, including state-of-the-art best practices for all open spaces and amenity areas.

"Memorials and Museums Master Plan"

- "One reason for distributing new memorials and museums throughout the city is to increase opportunities for direct spending on hotels, restaurants, shops, theaters and transportation."
- "New memorials and museums are to be encouraged in all quadrants of the city as a way of reinforcing local communities and local revitalization efforts."
- "Memorials and museums should support established land uses and local planning objectives."
- "Candidate Memorial/Museum Sites:
 - (a) South Capitol Street terminus at the Anacostia River (Florida Rock 'amenity' site [Sq. 664e])
 - (b) On the north shore of the Anacostia River, immediately east of the Douglass Bridge, SE

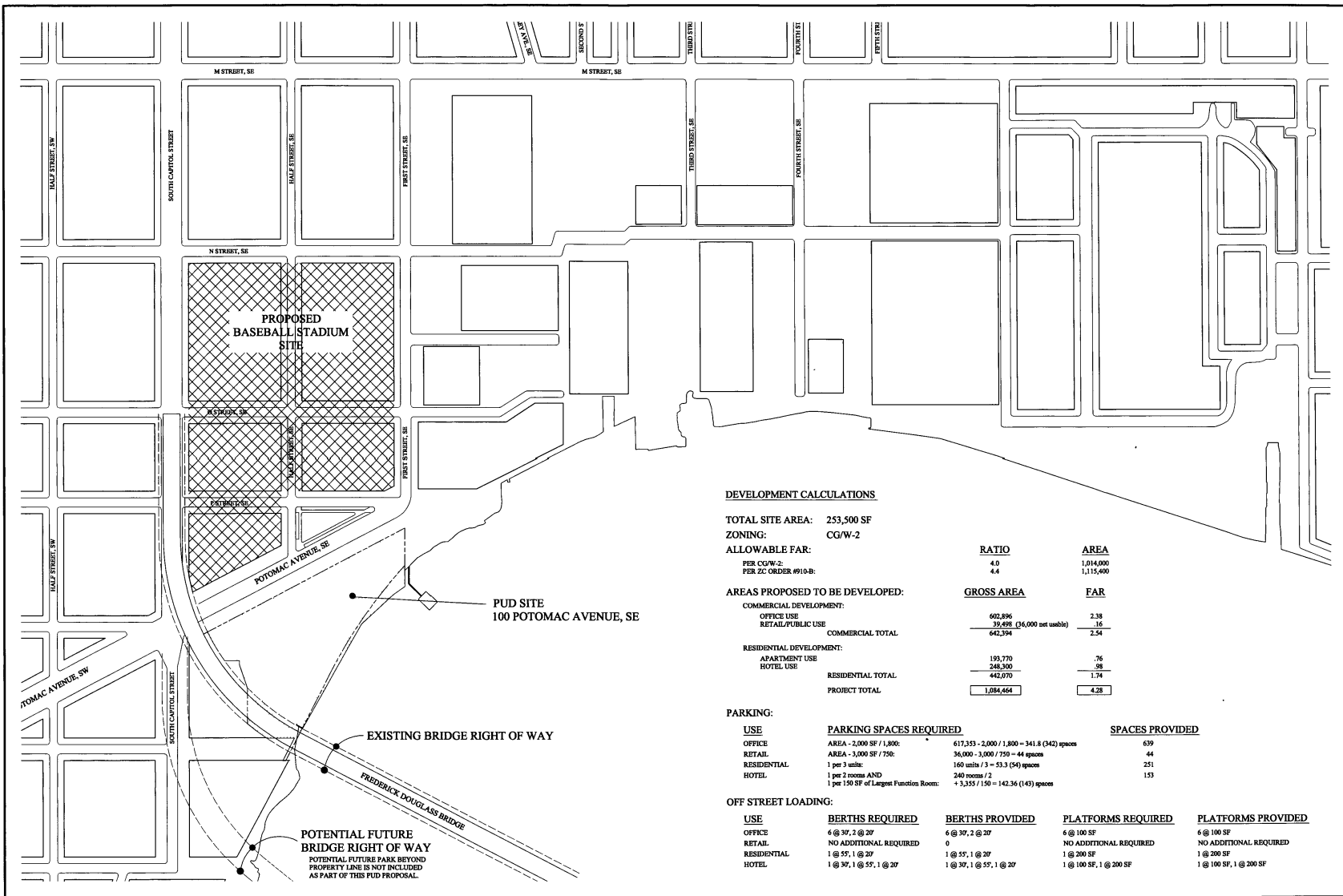
"Extending the Legacy"

- Regarding the Monumental Core: "...it is the economic center of Washington, where hundreds of thousands of people work and live."
- "Untangled from its maze of freeways and railroad tracks, South Capitol Street could serve as a new southern gateway to central Washington, at the scale of Pennsylvania Avenue, with a lively mix of shopping, housing and offices."
- Of the South Capitol and M Street area: "Thousands of new employees will require ... services that could spark the commercial rebirth of South Capitol and M Streets, SE."

SHEET #	SHEET TITLE	5/20/04 SUBMISSION	8/26/04 SUBMISSION	3/31/05 SUBMISSION
0	DRAWING MODIFICATION INDEX			NEW
1	VICINITY PLAN	NEW	REVISED	REVISED
2	EXISTING AND PROPOSED ZONING MAP	NEW	-	-
3	SITE SURVEY / EXISTING CONDITIONS	NEW	-	-
4	SEDIMENTATION AND EROSION CONTROL	NEW	-	-
5	SITE UTILITY AND SITE GRADING PLAN	NEW	-	-
6	SITE DRAINAGE PLAN	NEW	-	-
7	CIRCULATION PLAN AND BUILDING(S) DIMENSIONS	NEW	-	REVISED
8	LANDSCAPE KEY PLAN	NEW	-	REVISED
9	LANDSCAPE - PLANTING PLAN WEST	NEW	-	REVISED
9A	LANDSCAPE - AREA DETAIL PLANS			NEW
10	LANDSCAPE - PLANTING PLAN SOUTH	NEW	-	REVISED
10A	LANDSCAPE - PLANTING PLAN SOUTH PHASE II		NEW	REVISED
11	LANDSCAPE - PLANTING PLAN EAST	NEW	-	-
11A	LANDSCAPE - AREA DETAIL PLANS			NEW
12	LANDSCAPE - PLANTING PLAN AMENITY PARK	NEW	-	-
12A	LANDSCAPE - POTENTIAL FUTURE AREA PLAN		NEW	REVISED
12B	LANDSCAPE - BIOFILTRATION IMAGE PLAN			NEW
13	GROUND FLOOR PLAN	NEW	-	REVISED
14	THIRD FLOOR PLAN	NEW	-	REVISED
15	FIFTH FLOOR PLAN	NEW	-	REVISED
16	SEVENTH FLOOR PLAN	NEW	-	REVISED
17	NINTH FLOOR PLAN	NEW	-	REVISED
18	ROOF PLAN	NEW	-	REVISED
19	GARAGE FIRST FLOOR	NEW	-	REVISED
20	GARAGE SECOND FLOOR	NEW	-	REVISED

SHEET #	SHEET TITLE	5/20/04 SUBMISSION	8/26/04 SUBMISSION	3/31/05 SUBMISSION
21	GARAGE THIRD FLOOR	NEW	-	REVISED
22	EAST OFFICE BUILDING SECTION	NEW	-	-
23	EAST AND WEST OFFICE BUILDING SECTION	NEW	-	-
24	WEST OFFICE BUILDING SECTION	NEW	-	-
25	RESIDENTIAL BUILDING SECTION	NEW	-	-
26	PROJECT SQUARE ELEVATIONS	NEW	REVISED	REVISED
27	PROJECT SQUARE ELEVATIONS	NEW	REVISED	REVISED
28	HOTEL BUILDING ELEVATIONS	NEW	REVISED	REVISED
29	WEST OFFICE BUILDING ELEVATIONS	NEW	REVISED	REVISED
30	EAST OFFICE BUILDING ELEVATIONS	NEW	REVISED	-
31	EAST OFFICE BUILDING ELEVATIONS	NEW	REVISED	-
32	RESIDENTIAL BUILDING ELEVATIONS	NEW	REVISED	-
33	OFFICE BUILDING ELEVATION DETAILS	NEW	REVISED	REVISED
33A	EAST OFFICE BUILDING ELEVATION DETAILS			NEW
33B	WEST OFFICE BUILDING ELEVATION DETAILS			NEW
34	RESIDENTIAL / HOTEL ELEVATION DETAILS	NEW	REVISED	REVISED
34A	RESIDENTIAL BUILDING ELEVATION DETAILS			NEW
34B	HOTEL BUILDING ELEVATION DETAILS			NEW
35	RETAIL LOCATION PLAN	NEW	-	REVISED
36	RENDERING - WEST WATERFRONT ESPLANADE			NEW
37	RENDERING - WATERFRONT TERRACE			NEW
38	RENDERING - CENTER WATERFRONT ESPLANADE			NEW
39	RENDERING - DIAGONAL ALLEE			NEW
40	RENDERING - HALF STREET EXTENSION			NEW
41	RENDERING - HOTEL CAFE,			NEW
42	MATERIAL SAMPLES			NEW





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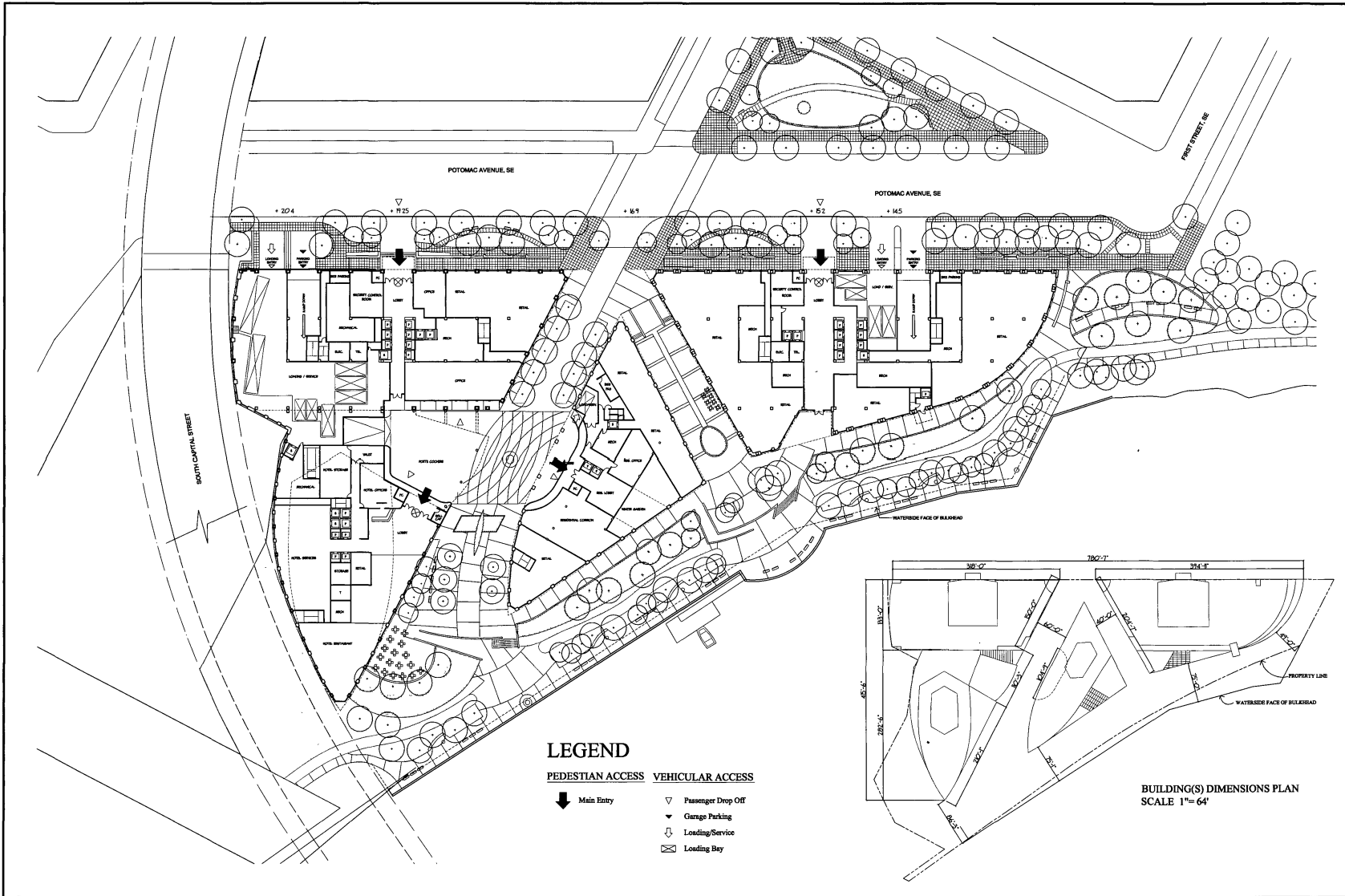
VICINITY PLAN
ZONING COMMISSION CASE NO. 01-317E/98-17E95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2005

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CIRCULATION PLAN AND BUILDING(S) DIMENSIONS
ZONING COMMISSION CASE NO. 01-31TE98-17E95-16P
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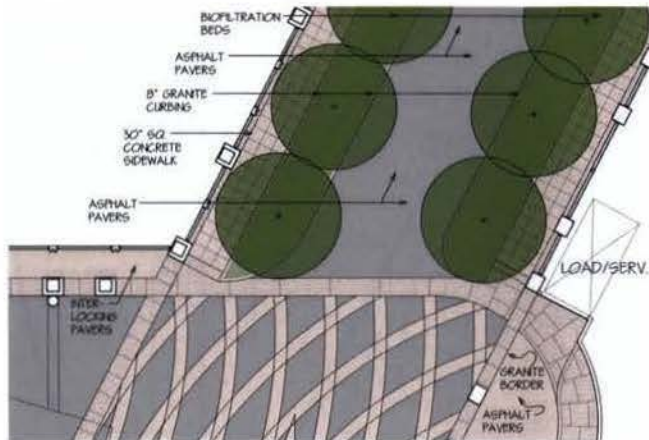
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PRE-HEARING SUBMISSION
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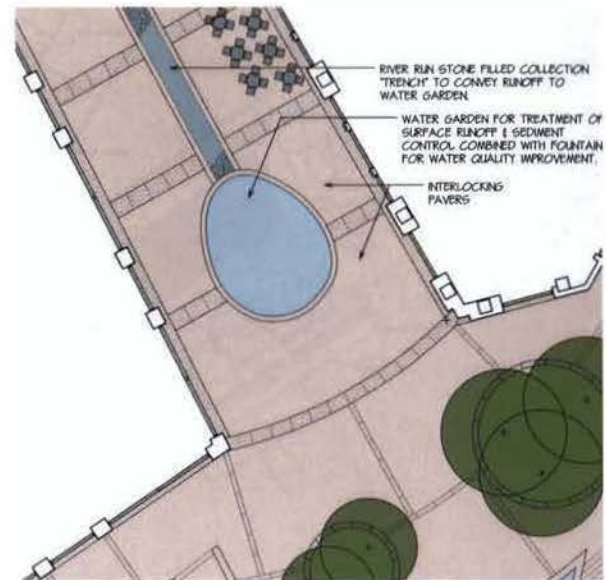
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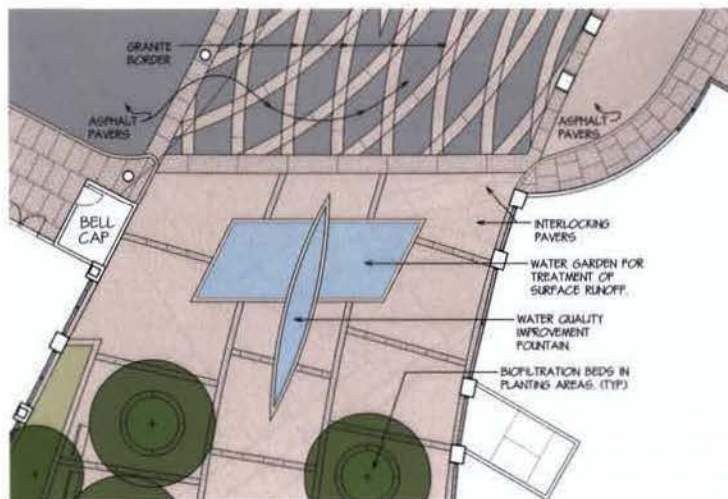




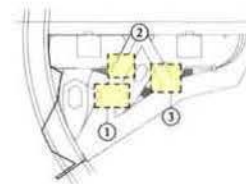
2 DETAIL PLAN

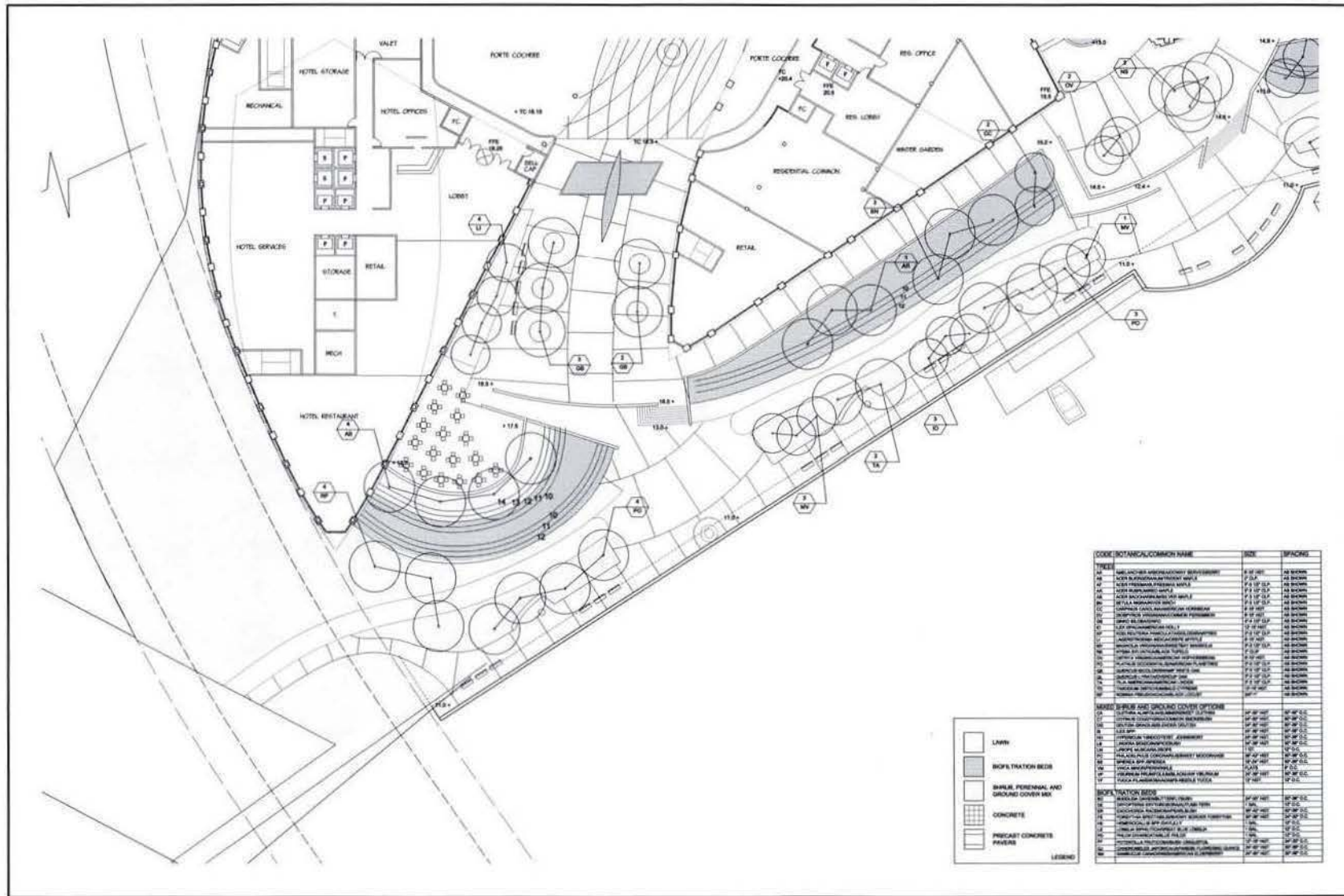


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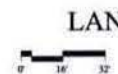


1 DETAIL PLAN





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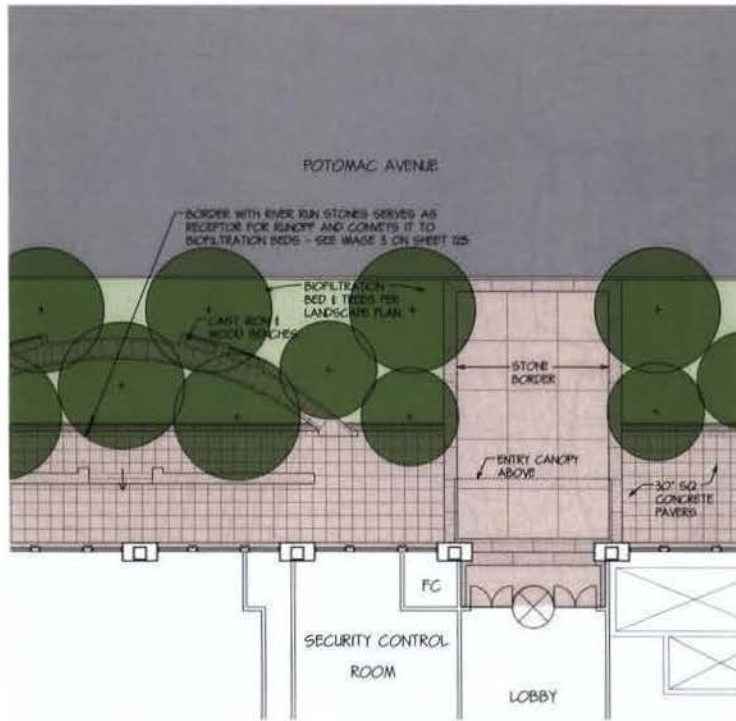
LANDSCAPE - PLANTING PLAN SOUTH

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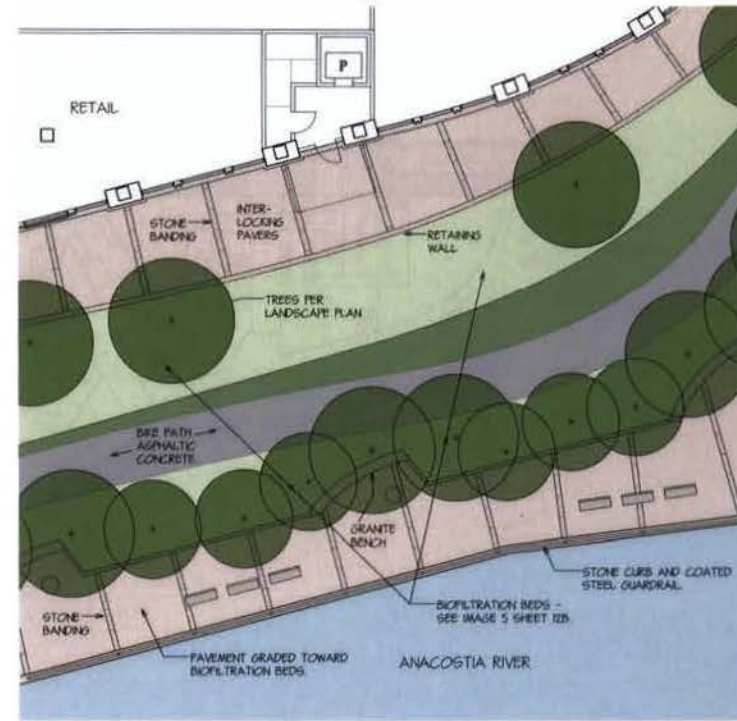
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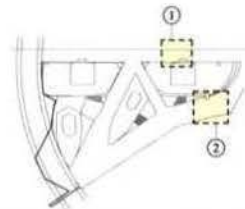
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1 DETAIL PLAN



2 DETAIL PLAN



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LANDSCAPE - AREA DETAIL PLANS

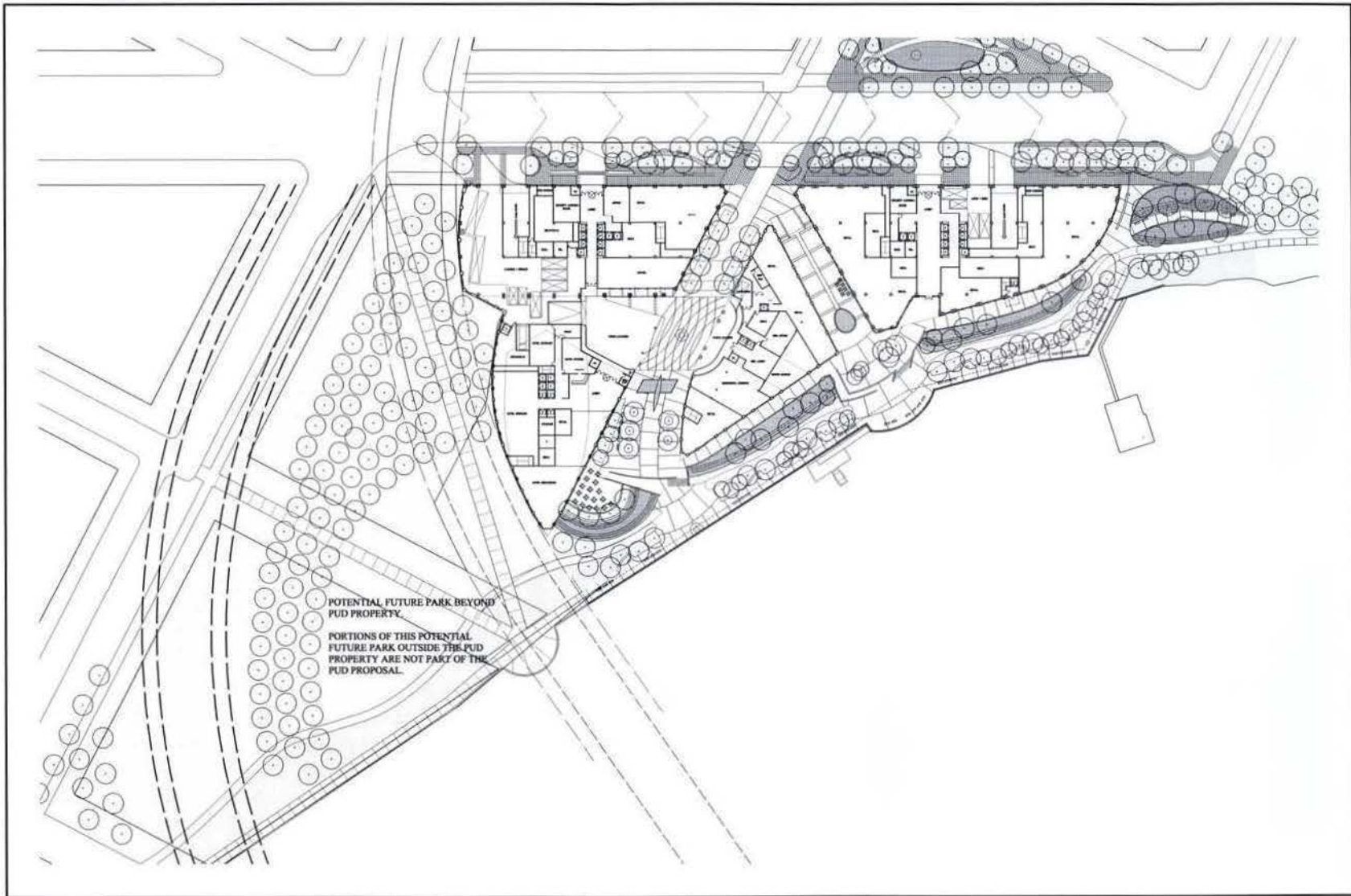
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LANDSCAPE - POTENTIAL FUTURE AREA PLAN
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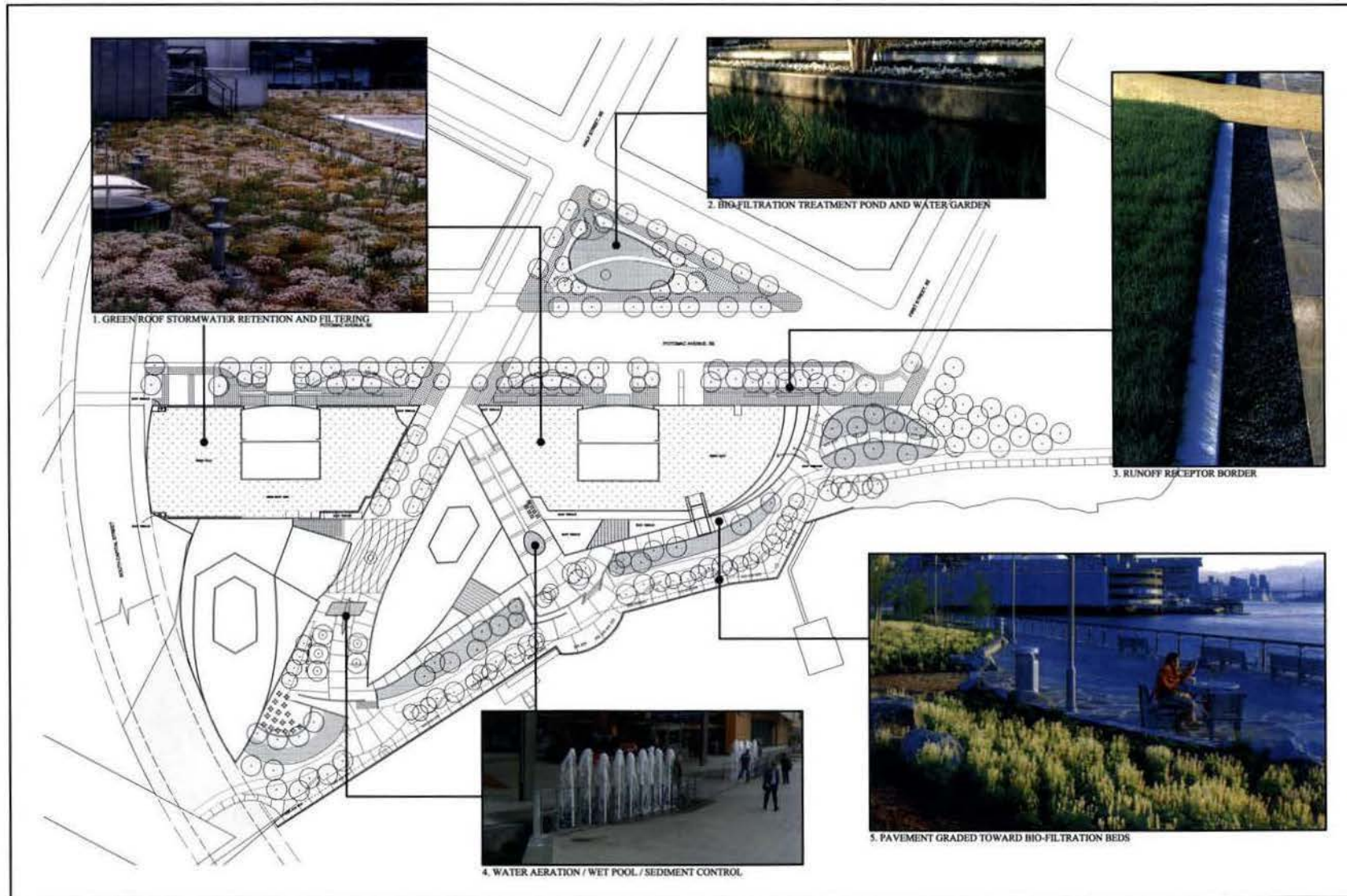


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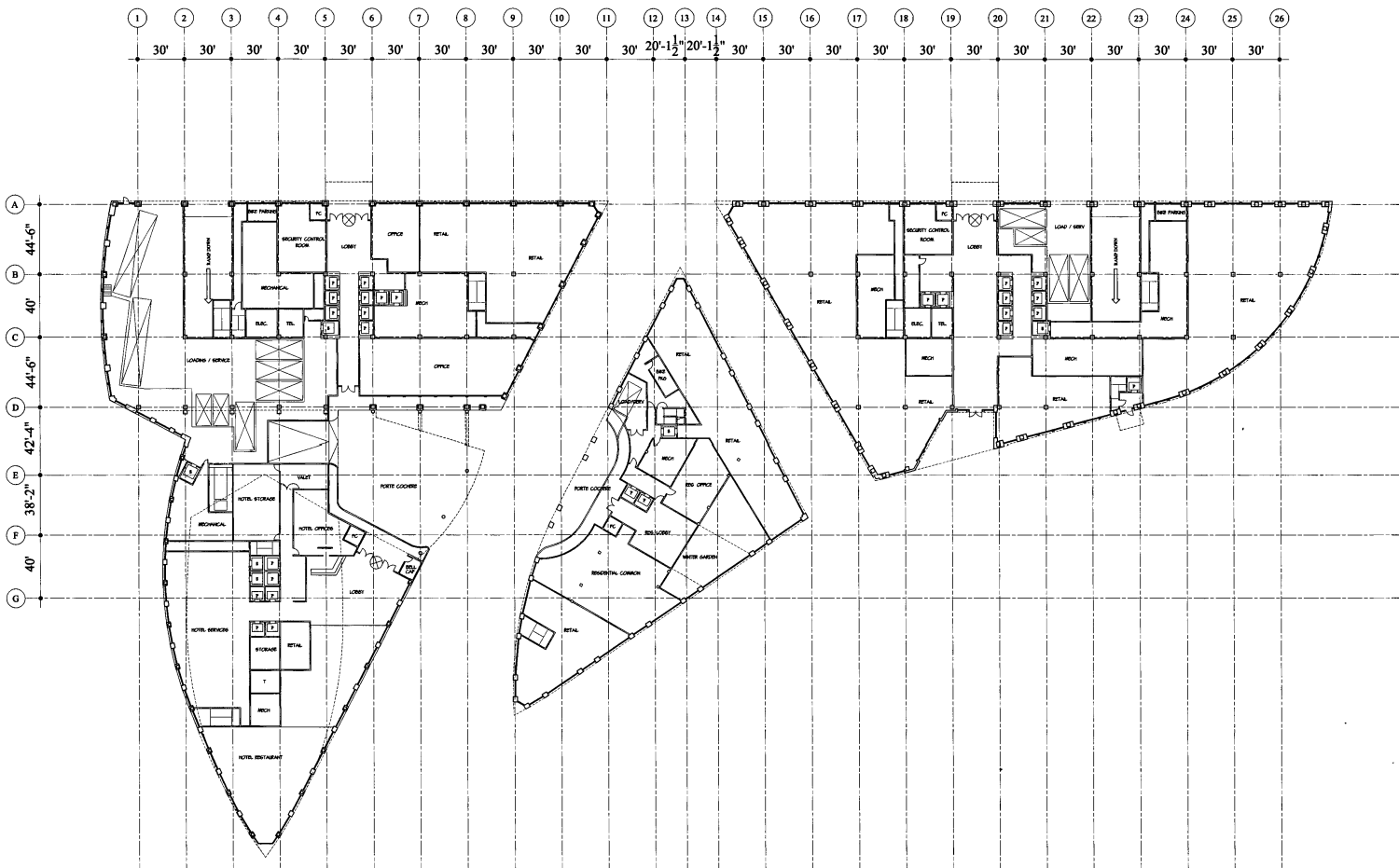
LANDSCAPE - BIOFILTRATION IMAGE PLAN

ZONING COMMISSION CASE NO. 01-311E/98-17/95-16P
100 POTOMAC AVENUE, SE

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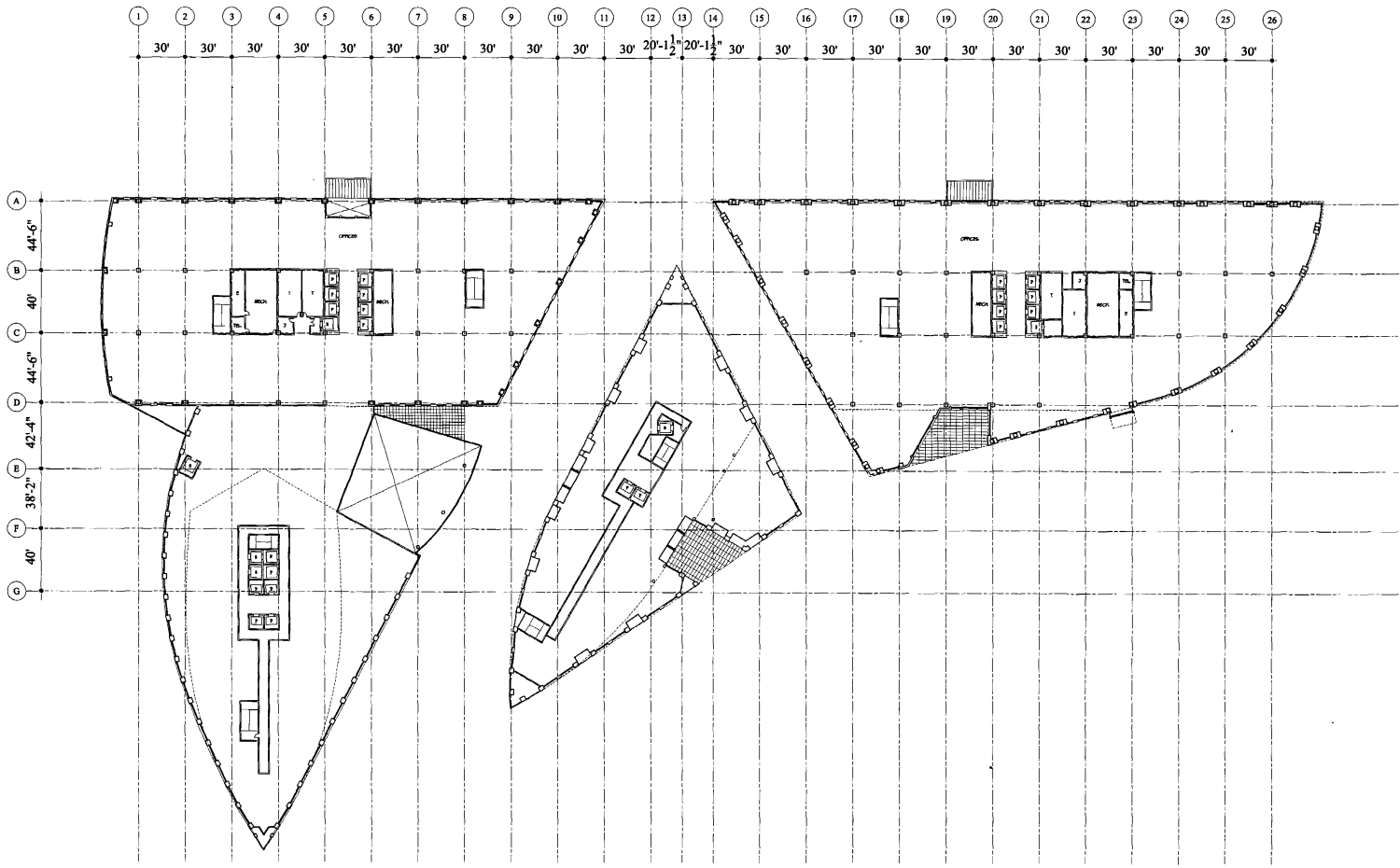
GROUND FLOOR PLAN
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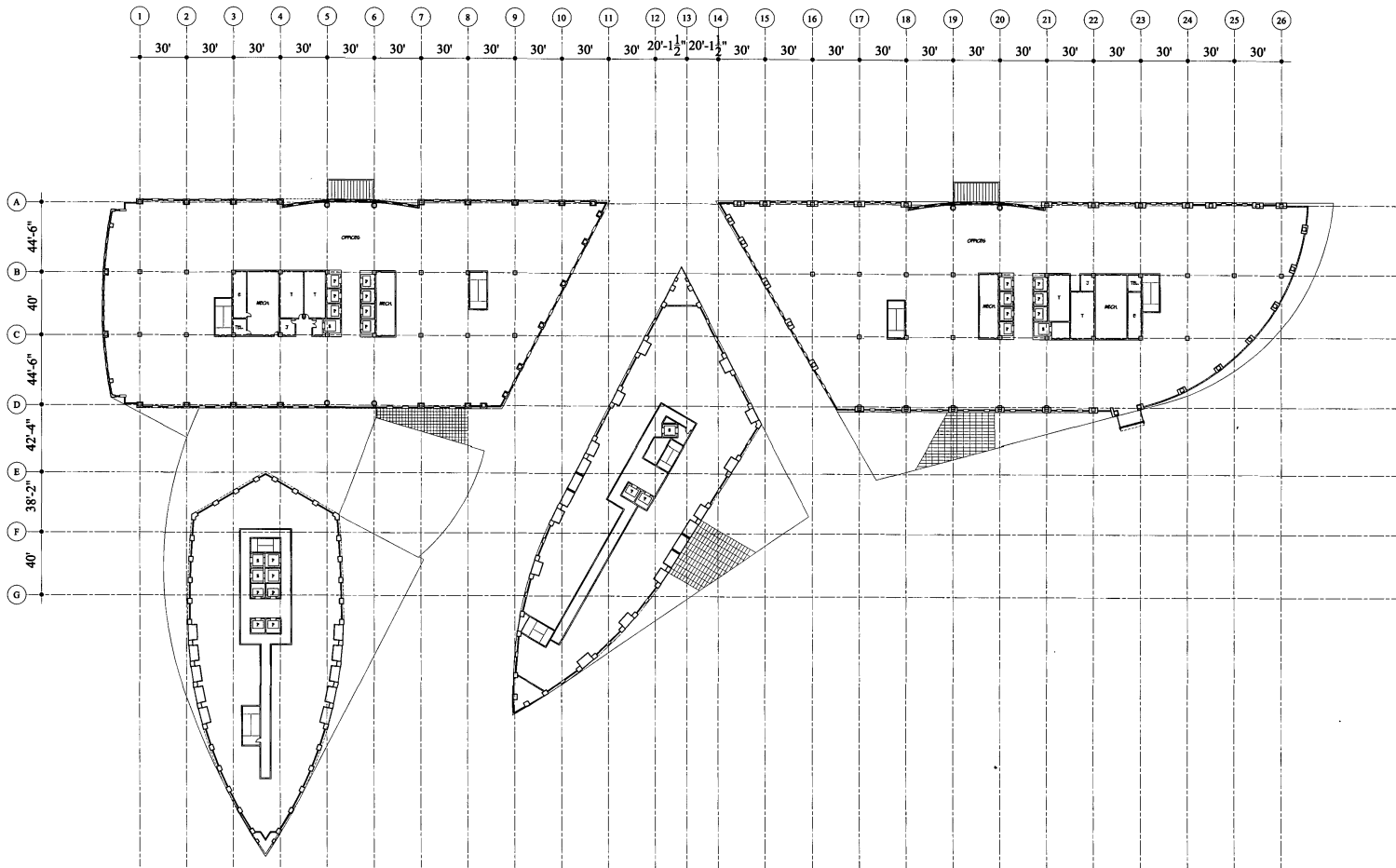
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PRE-HEARING SUBMISSION
APRIL 14, 2005

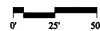
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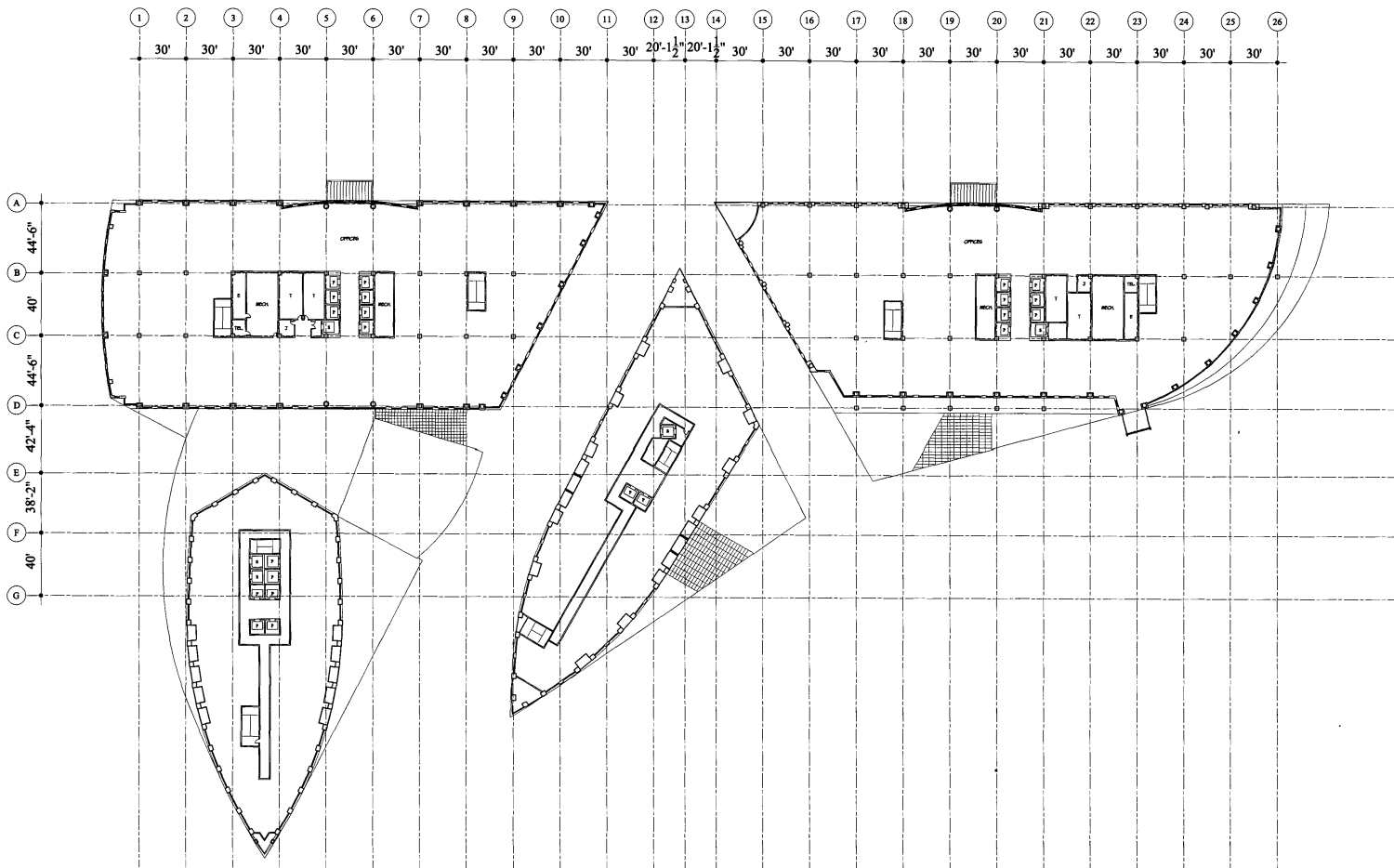
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100 POTOMAC AVENUE, SE

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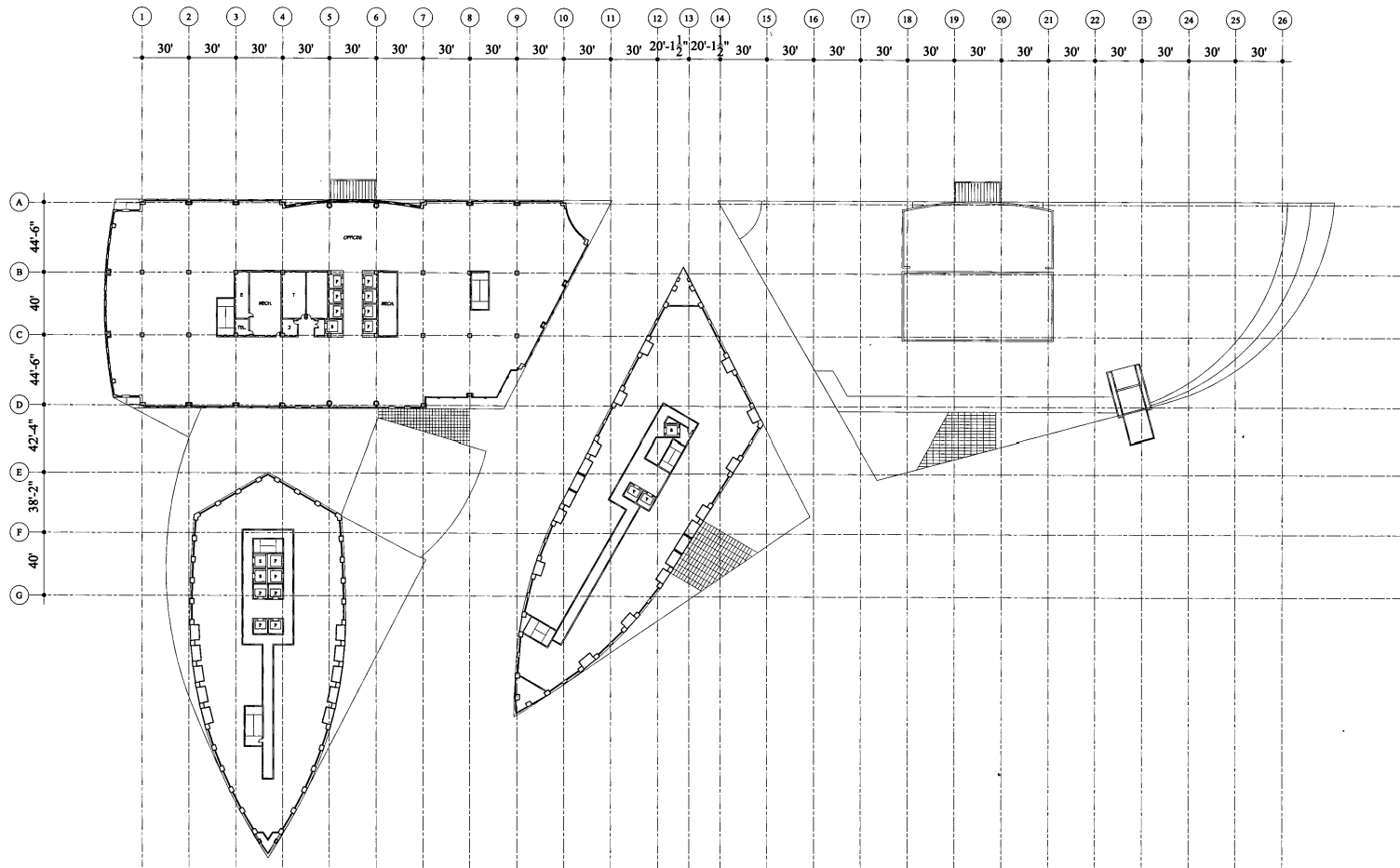
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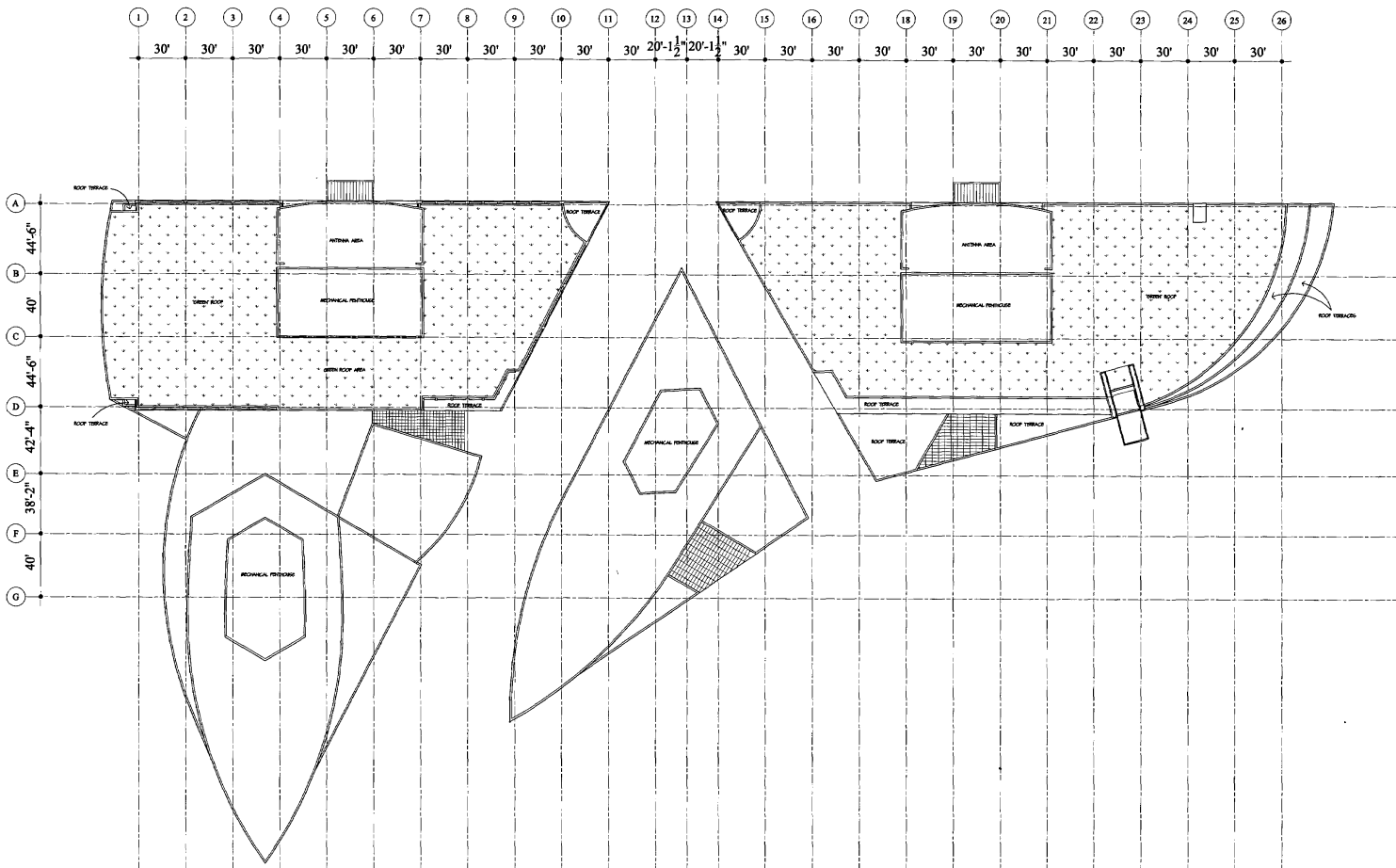
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PRE-HEARING SUBMISSION
APRIL 14, 2005

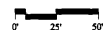
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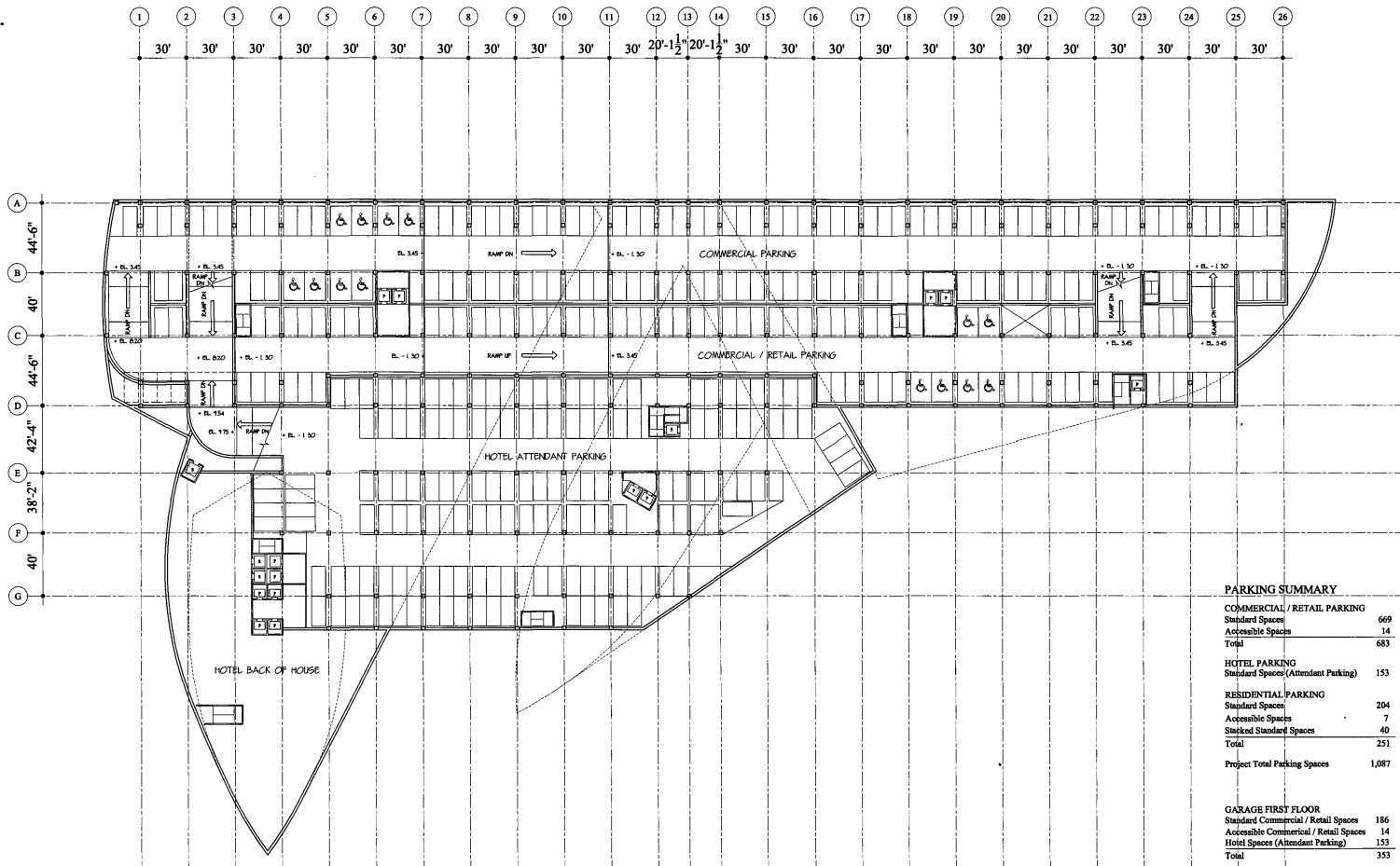
ROOF PLAN
ZONING COMMISSION CASE NO. 01-31TE98-17E95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
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PARKING SUMMARY

COMMERCIAL / RETAIL PARKING	
Standard Spaces	669
Accessible Spaces	14
Total	683

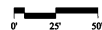
HOTEL PARKING	
Standard Spaces (Attendant Parking)	153

RESIDENTIAL PARKING	
Standard Spaces	204
Accessible Spaces	7
Stacked Standard Spaces	40
Total	251

Project Total Parking Spaces 1,087

GARAGE FIRST FLOOR	
Standard Commercial / Retail Spaces	186
Accessible Commercial / Retail Spaces	14
Hotel Spaces (Attendant Parking)	153
Total	353

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GARAGE FIRST FLOOR PLAN

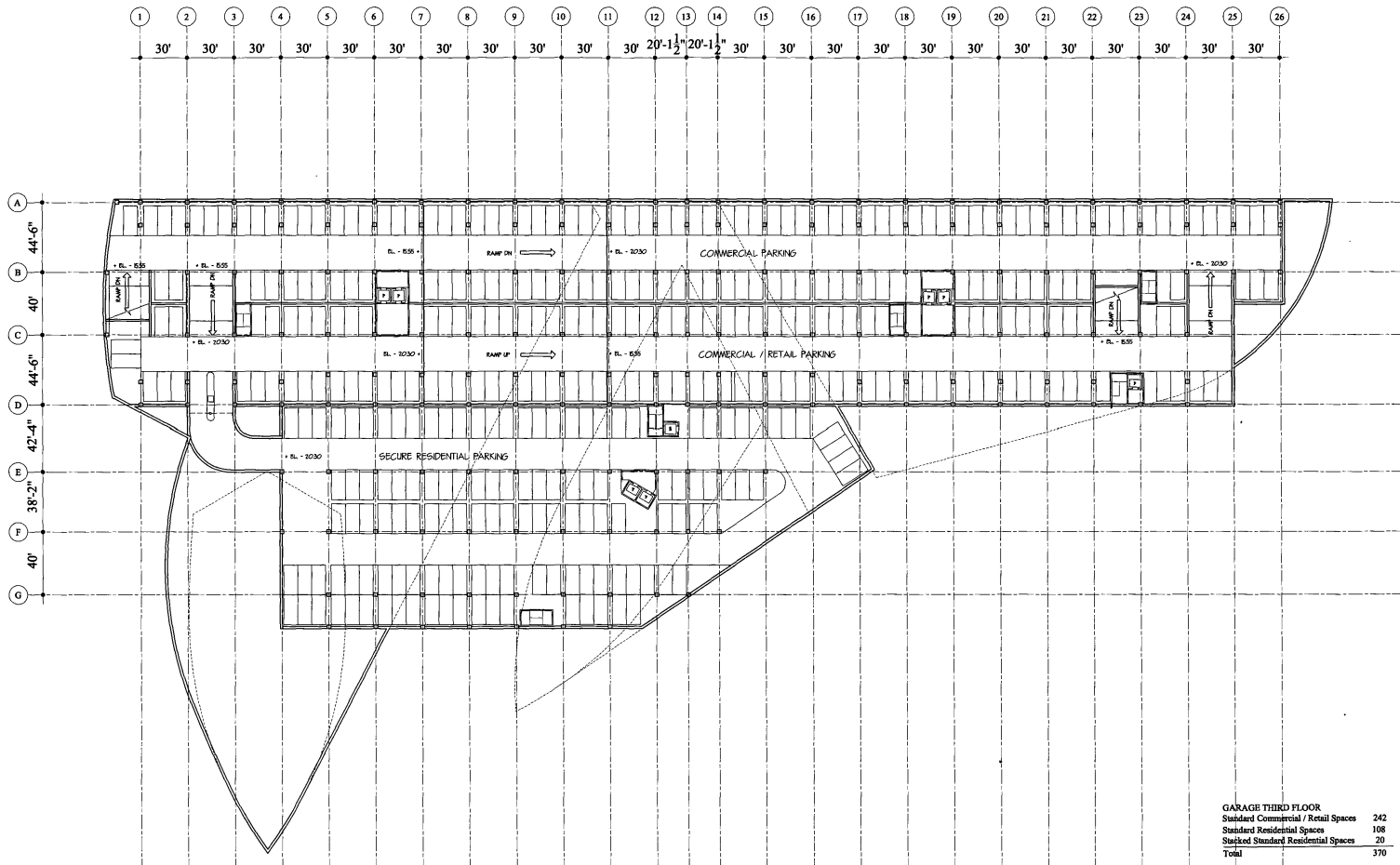
ZONING COMMISSION CASE NO. 01-31TE98-17P95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2005

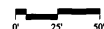
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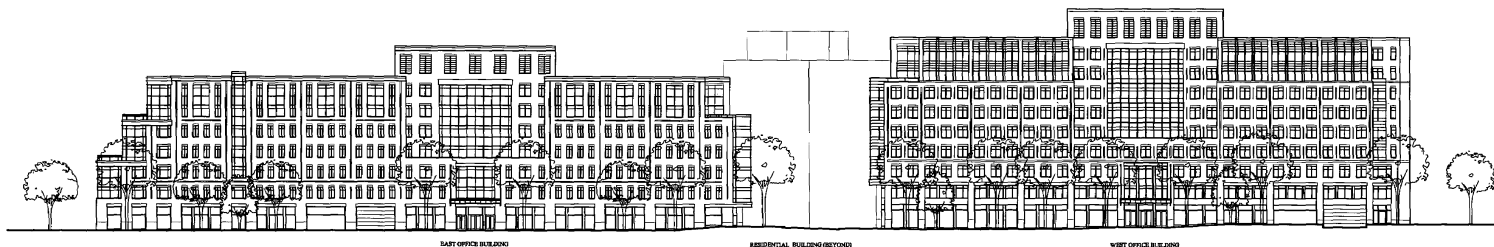
GARAGE THIRD FLOOR PLAN

ZONING COMMISSION CASE NO. 01-31TE98-17F95-16P
100 POTOMAC AVENUE, SE

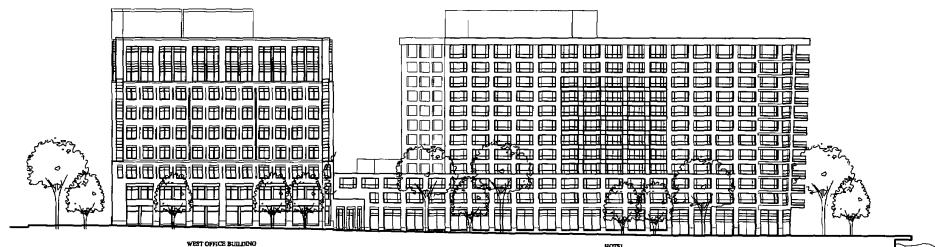
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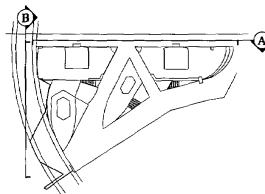
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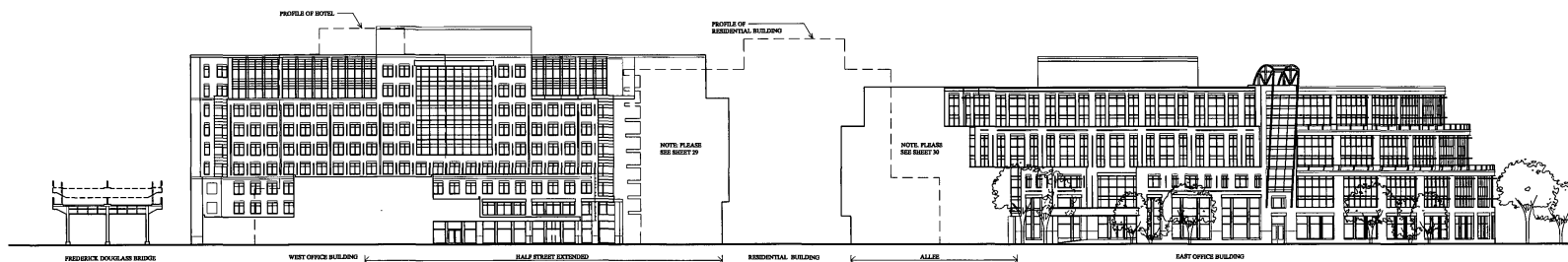


A. POTOMAC AVENUE NORTH ELEVATION

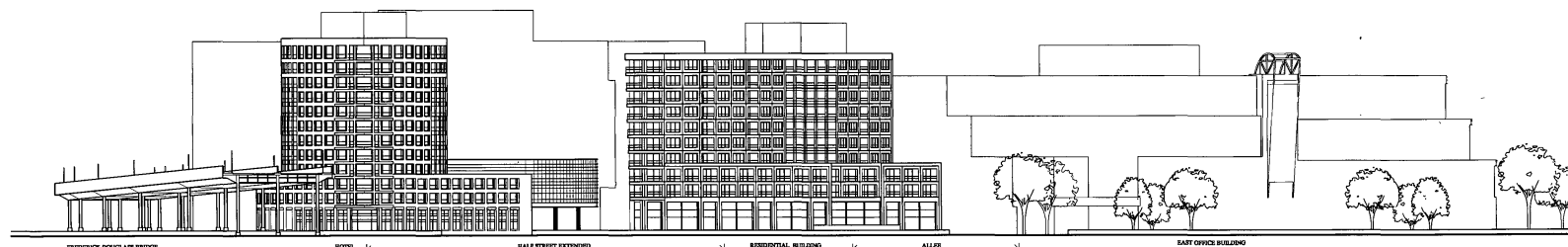


B. WEST ELEVATION

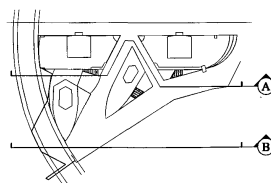




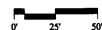
A. ANACOSTIA RIVER SOUTH ELEVATION



B. ANACOSTIA RIVER SOUTH ELEVATION



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PROJECT SQUARE ELEVATIONS

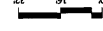
ZONING COMMISSION CASE NO. 01-311TE/98-17E/95-16P
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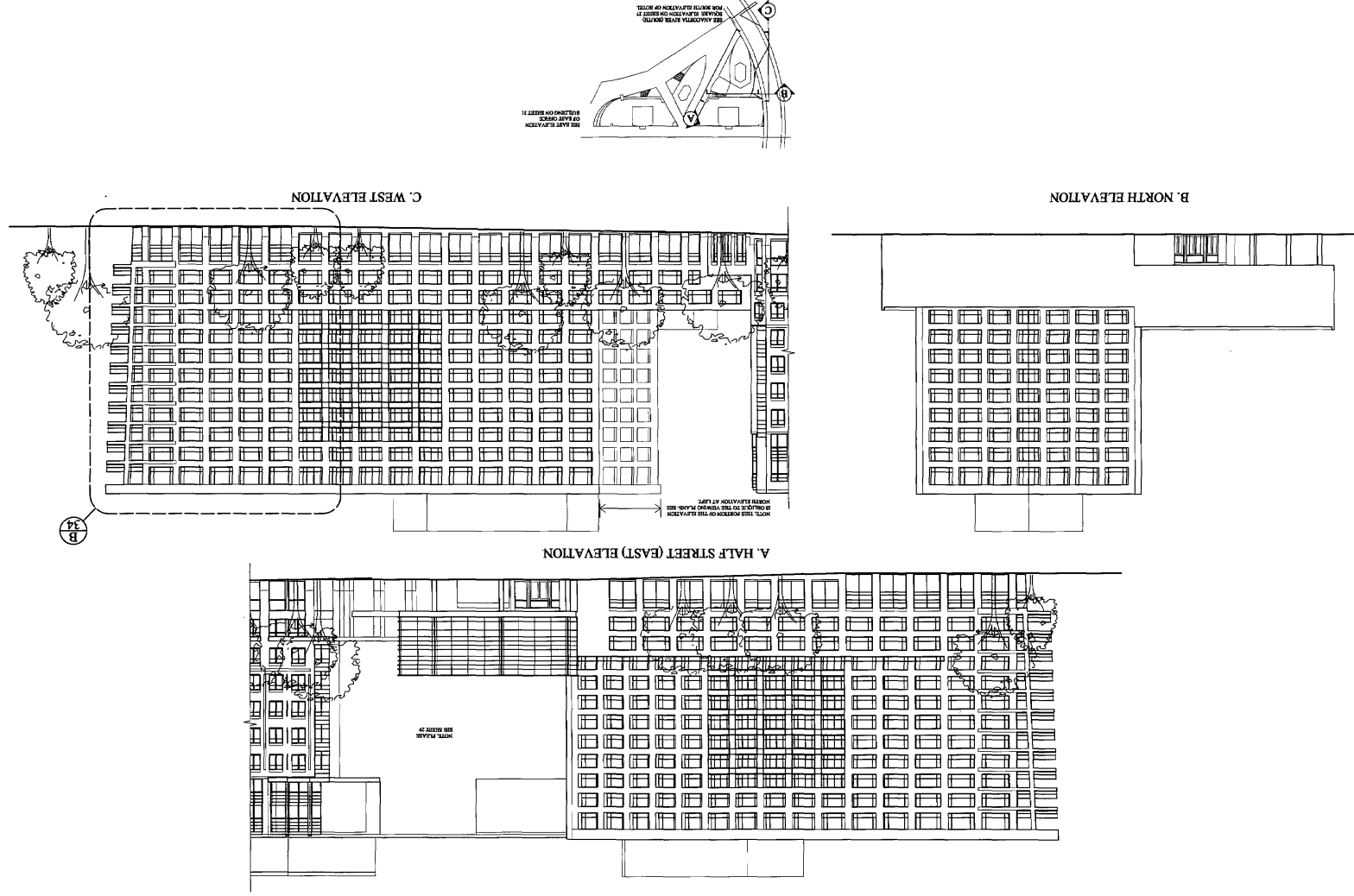
HOTEL BUILDING ELEVATIONS
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100 POTOMAC AVENUE, SE

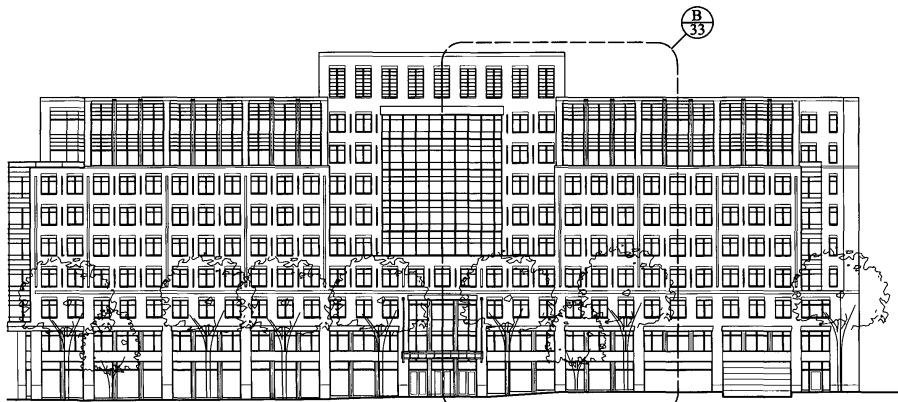
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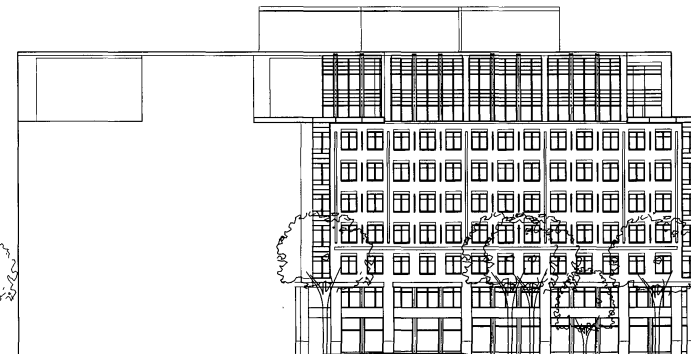
A. POTOMAC AVENUE (NORTH) ELEVATION



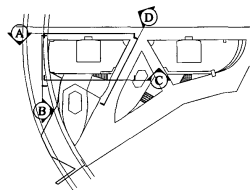
B. WEST ELEVATION



C. ANACOSTIA RIVER (SOUTH) ELEVATION



D. HALF STREET (EAST) ELEVATION



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WEST OFFICE BUILDING ELEVATIONS

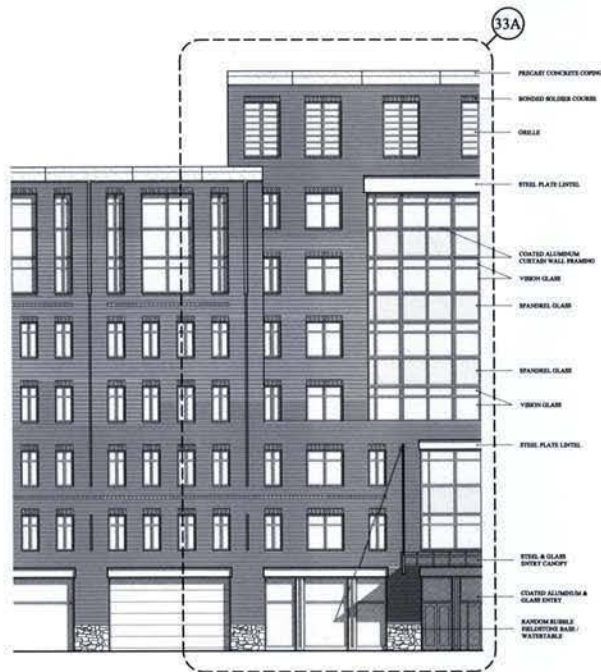
ZONING COMMISSION CASE NO. 01-31TE98-17F95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2005

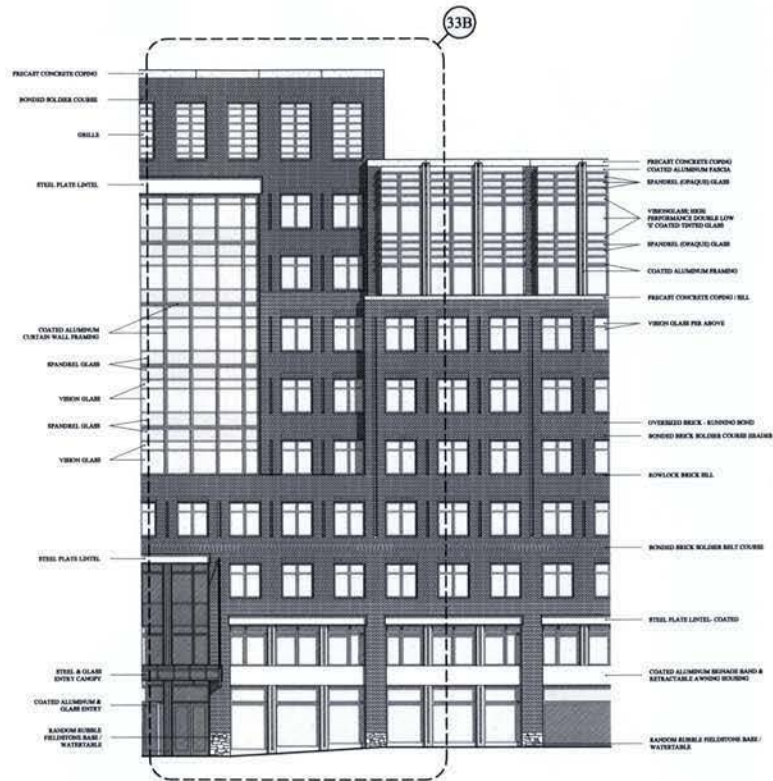
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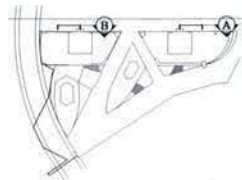
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A. EAST OFFICE BUILDING DETAIL



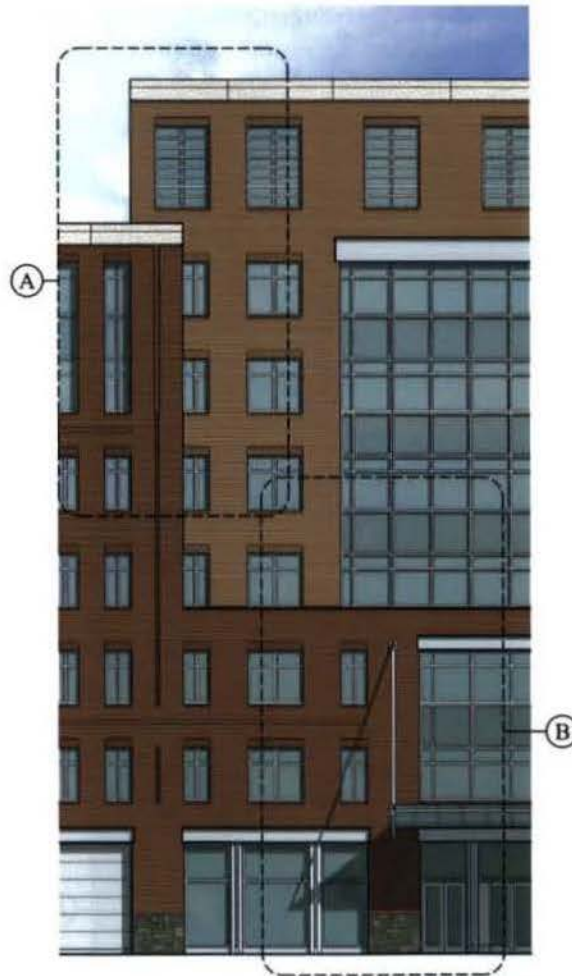
B. WEST OFFICE BUILDING DETAIL





DETAIL A

SCALE: 3/8" = 1'-0"



SCALE: 3/16" = 1'-0"



DETAIL B

SCALE: 3/8" = 1'-0"

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EAST OFFICE BUILDING ELEVATION DETAILS

ZONING COMMISSION CASE NO. 01-317E/98-17/95-1AP
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APRIL 14, 2005

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DETAIL A

SCALE: 3/8" = 1'-0"



SCALE: 3/16" = 1'-0"



DETAIL B

SCALE: 3/8" = 1'-0"

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WEST OFFICE BUILDING ELEVATION DETAILS

ZONING COMMISSION CASE NO. 01-31TE/08-177/05-10P

100 POTOMAC AVENUE, SE

PRE-HEARD/01 SUBMISSION
APRIL 14, 2005

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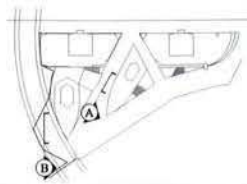
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A. RESIDENTIAL ELEVATION DETAIL



B. HOTEL ELEVATION DETAIL





DETAIL A

SCALE: 3/8" = 1'-0"



SCALE: 3/16" = 1'-0"



DETAIL B

SCALE: 3/8" = 1'-0"

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RESIDENTIAL BUILDING ELEVATION DETAILS
ZONING COMMISSION CASE NO. 01-311E/06-17/05-14P
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APRIL 14, 2005



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DETAIL A

SCALE: 3/8" = 1'-0"



A



B

SCALE: 3/16" = 1'-0"



DETAIL B

SCALE: 3/8" = 1'-0"

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HOTEL BUILDING ELEVATION DETAILS

ZONING COMMISSION CASE NO. 01-31TE/98-177/95-14P

100 POTOMAC AVENUE, SE

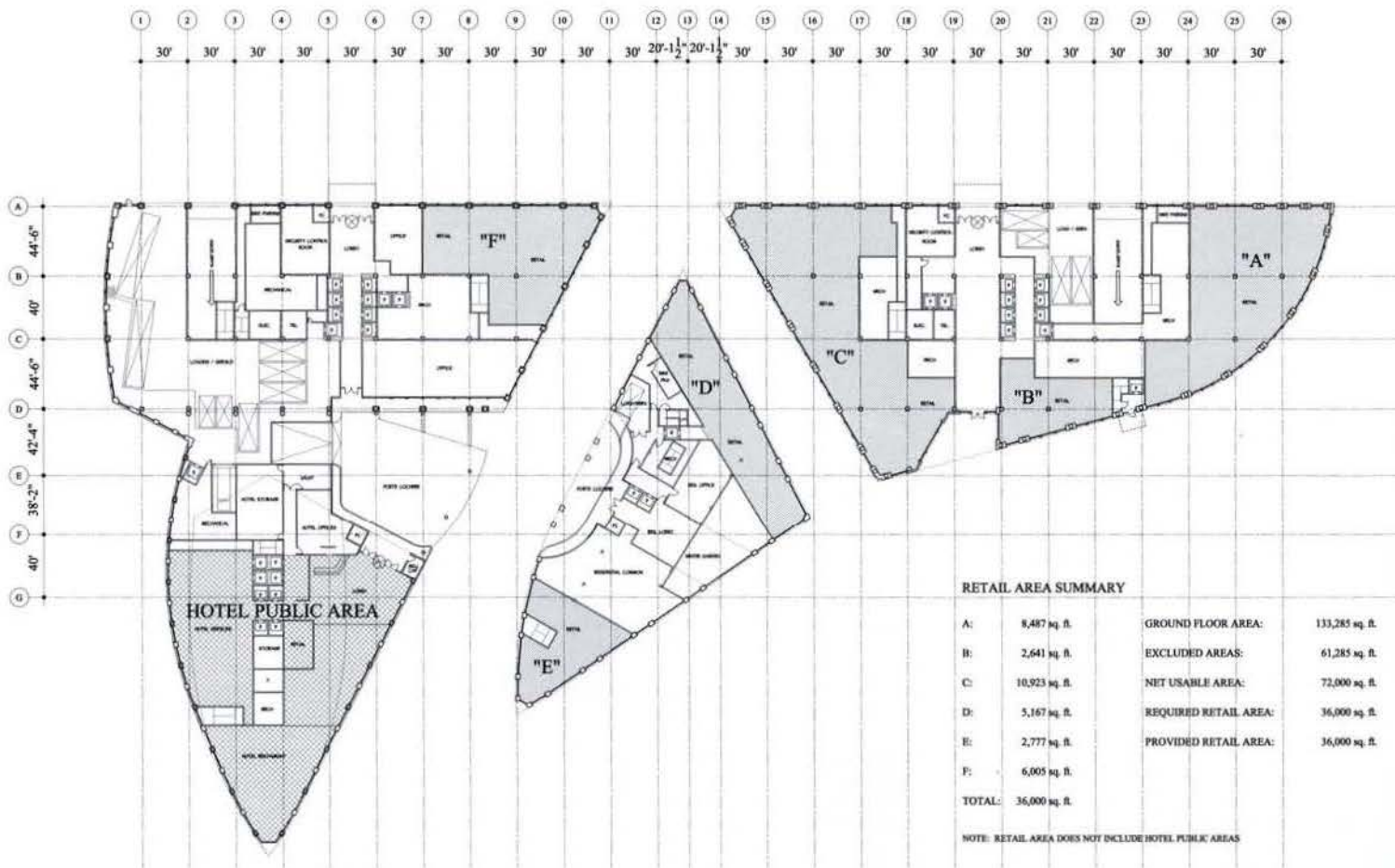
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RETAIL LOCATION PLAN
ZONING COMMISSION CASE NO. 01-31TE/98-17/95-16P
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APRIL 14, 2005

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VIEWS ALONG THE ANACOSTIA RIVER ESPLANADE LOOKING NORTHEAST TOWARD THE NAVY YARD AND NORTH UP THE HALF STREET CORRIDOR TO POTOMAC AVENUE.

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RENDERING - WEST WATERFRONT ESPLANADE
ZONING COMMISSION CASE NO. 01-31TE/98-17F/95-14P
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PRE-HEARING SUBMISSION
APRIL 14, 2005

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VIEW ALONG THE UPPER TERRACE OF THE ANACOSTIA WATERFRONT ESPLANADE LOOKING EAST TOWARD THE NAVY YARD UP RIVER.

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RENDERING - WATERFRONT TERRACE

ZONING COMMISSION CASE NO. 01-317E/96-177/95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2005

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PHOTO: JAMES BUCKLEY, A. JAMES BUCKLEY, A. JAMES BUCKLEY, A.



VIEW FROM THE UPPER TERRACE AT THE SOUTH END OF PEDESTRIAN ALEE LOOKING SOUTHWEST ALONG THE ESPLANADE TO THE WATER TAXI.

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RENDERING - CENTER WATERFRONT ESPLANADE
ZONING COMMISSION CASE NO. 01-3110/08-17/95-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2009

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THE CITY OF SPARKS, MARYLAND



LOOKING FROM THE ESPLANADE TERRACE LEVEL TO A VIEW OF THE PEDESTRIAN SHOPS ALLEE, THE RESIDENTIAL BUILDING TO THE LEFT, AND EAST OFFICE BUILDING TO THE RIGHT, WITH POTOMAC AVENUE AND HALF STREET INTERSECTION BEYOND.

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RENDERING - DIAGONAL ALLEE
ZONING COMMISSION CASE NO. 01-317E/06-17F/05-16P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
APRIL 14, 2005

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RENDERING BY DAVIS BUCKLEY ARCHITECTS & PLANNERS



VIEW OF PLAZA AT THE END OF HALF STREET VIEW CORRIDOR SHOWING BIO-FILTRATION "WET-POOL" WITH THE ESPLANADE, THE ANACOSTIA RIVER, AND THE EXISTING FREDERICK DOUGLASS BRIDGE BEYOND.

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RENDERING - HALF STREET EXTENSION

ZONING COMMISSION CASE NO. 01-31TE/98-17F/95-16P
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VIEW OF OUTDOOR DINING AREA LOOKING TOWARD THE ANACOSTIA RIVER WITH THE HOTEL IMMEDIATELY TO THE RIGHT.

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RENDERING - HOTEL CAFE
ZONING COMMISSION CASE NO. 01-311E/98-17795-14P
100 POTOMAC AVENUE, SE

PRE-HEARING SUBMISSION
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